

# WARNERS BAY STREETScape MASTER PLAN

Issue 01 Version 02 Mar 2017



Belmont

Glendale

Swansea

Toronto

Morisset

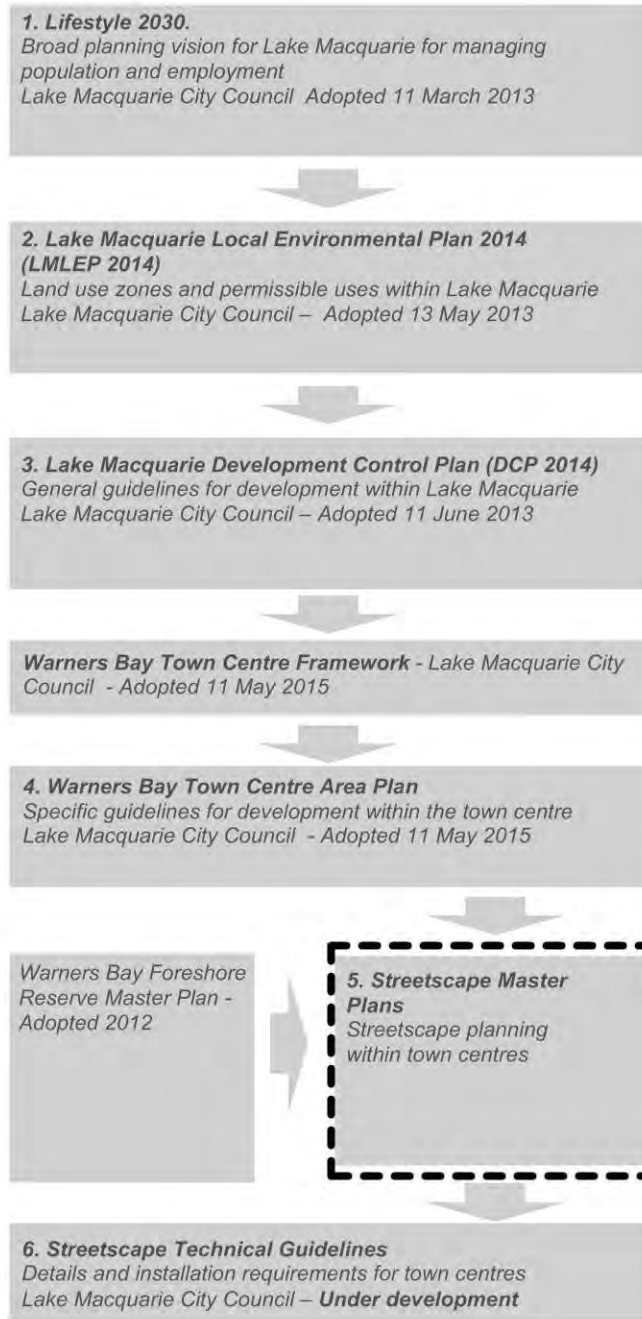
Cardiff

Mount Hutton

Charlestown







Note: the planning documents listed above are current at the time of adoption of this Streetscape Master Plan. When using these documents please confirm that you have the latest versions.

Issue Ver.	Date	Notes
01 00	11.5.15	Adopted by Council
01 02	March 2017	Revisions following the creation of the Streetscape Technical Guidelines

**Owner:** Asset Management  
**Users:** All Council staff, the community and developers.  
**Prepared by:** City Projects

## LAKE MACQUARIE TOWN CENTRE STREETScape MASTER PLANS

The 2008/2009 Management Plan and Budget identified the need to provide high quality public domains and a city identity. The Streetscape Master Plans provide a document that identifies this vision and provides a plan for its implementation. The Streetscape Master Plans are intended to improve the aesthetic appearance of the town centres, promote the areas personality, increase opportunity, accessibility, functionality, economic effect and provide a modern easily maintainable public domain.

The Streetscape Master Plans ensure a unified approach to undertaking public domain works by Council, developers and the community. It also assists in directing public and private investments and will inform the Chamber of Commerce in the preparation of strategic plans.

The Streetscape Master Plans are substantially focused on the areas **subject to Area Plans under Lake Macquarie LEP 2014**. These are the areas of each of the town centres where the majority of commercial activity will occur. These areas are also the most likely to undergo change in the future.

The consideration and collation of all relevant issues resulted in this overall priority order for the Streetscape Master Plans:

1. Charlestown – completed and adopted by Council
2. Cardiff – completed and adopted by Council
3. Belmont – completed and adopted by Council
4. Toronto – completed and adopted by Council
5. Morisset – completed and adopted by Council
6. Warners Bay – completed and adopted by Council
7. Swansea
8. Mount Hutton
9. Glendale

The priority order adopted by Council has been adjusted for the 2014/2015 financial year to prioritise Mount Hutton ahead of Glendale (as shown above) due to the impact of the Lake Macquarie Transport Interchange on the Glendale Town Centre.



### OBJECTIVES OF TOWN CENTRE STREETScape MASTER PLANS

- To provide a clear vision and identity for each town centre
- To provide a clear guide for Council, the community and developers
- To create vibrant active town centres
- To provide consistencies within Lake Macquarie
- To preserve and enhance the uniqueness of Lake Macquarie's town centres
- To ensure high quality outcomes
- To reduce asset maintenance costs

### EXTENT OF THE WARNERS BAY STREETScape MASTER PLANS

Generally aligned with the *Warners Bay Area Plan*, the areas covered by the *Streetscape Master Plan* are the public areas owned and /or managed by Council. The focus is on the streetscape elements within the road reserve and include street trees, tree guards, furniture, bins, bike racks, drinking fountains, bollards, lighting, paving, public art and town centre signage. For consistency the streetscape treatments should be extended onto private land at building setbacks, building forecourts, arcade entrances, pedestrian lanes etc. and where logical to address pedestrian links. New developments need to address the streetscape as prescribed by this plan. Items and selections nominated in this plan will be reviewed periodically by LMCC in accordance with relevant legislation.

### TOWN CENTRE PERSONALITIES

Town centre personalities have been developed for the nine main town centres. These have been derived to provide a strong town centre identity and theme for enhancing character, heritage and sense of place.

- The nine main town centres personalities are:
- Charlestown - the Central Business District
  - Cardiff - the Skills & Trades Centre
  - Belmont - the Activity Centre
  - Toronto - the Lakeside Village
  - Morisset - Nurtures the Spirit of Community
  - Warners Bay - the Lifestyle Centre**
  - Glendale - the Dynamic Services Hub
  - Swansea - the Seaside Village
  - Mount Hutton - the Growing Community



## AIMS OF THE WARNERS BAY STREETScape MASTER PLAN

- To build on Warners Bay identity as a retail, commercial and recreational centre.
- To create zones for outdoor dining, particularly on The Esplanade.
- To visually and physically connect the town centre with the lake foreshore.
- To respond to and complement the building controls contained within the *Warners Bay Town Centre Area Plan* to create a relationship between the built form of the town centre buildings and the public domain.
- To provide a palette of materials and finishes for streetscape elements that create clean lines and reduce visual clutter.

## VISION FOR WARNERS BAY STREETScape MASTER PLAN

- To create a vibrant town centre
- To enhance safety and accessibility
- To create a walkable connected town centre
- To emphasise and enhance the link between the town centre and its foreshore.
- To provide a sustainable environment
- To provide opportunities for public art
- To provide high quality furniture, lighting and paving
- To enhance the character of surrounding streets
- To promote walking, cycling and public transport
- Use of water sensitive urban design principles to minimise stormwater impacts and improve street amenity.
- To activate the precinct as a safe and inviting destination

## CONSTRAINTS

- Services and awnings constrain tree planting opportunities.
- Past development and maintenance has led to varied pavement styles.
- Roads & Maritime Services (RMS) requirements will be applicable for state managed roads.

## ADDITIONAL PLANNING CONSIDERATIONS FOR THE WARNERS BAY STREETScape MASTER PLAN:

**NOTE:**  
Refer page 1 for general planning controls.

- Warners Bay Town Centre Framework
- Warners Bay Town Centre Area Plan
- Warners Bay Foreshore Reserve Master Plan
- LMCC Cycling Strategy 2021
- LMCC Public Art Strategy
- LMCC Draft Footpath Dining Policy
- LMCC Footpath Strategy

These documents have been reviewed and considered as part of this plan. Where these initiatives or any others are adopted or modified the Streetscape Master Plan should be updated to reflect this.



## Legend

- Town centre signage
- Existing Bus Stop location
- Boundary- Streetscape Master Plan
- Boundary- Town Centre under the Area Plan
- Existing cycle route
- Proposed cycle route (LMCC Cycling Strategy)
- Proposed cycle route (this Master Plan)
- Great North Walk - route through Warners Bay Town Centre
- Dress Circle streetscape
- Dress Circle with off-road cycleway
- Town Centre Core streetscape
- Mixed Use streetscape
- Mixed Use with shared path
- Residential streetscape
- Postman's Lane Shared Zone
- Lymington Way
- Existing pedestrian link
- Existing open pedestrian arcade
- Proposed open pedestrian arcade
- Existing through-building pedestrian arcade
- Proposed through-building pedestrian arcade
- Public square
- John St Oval
- Public Playground
- Performing Arts Centre
- Warners Bay Foreshore Reserve
- Post Office
- St Marys Primary School
- Warners Bay Bowling Club
- Feighan Oval

Foreshore area is subject to a separate master plan- see Warners Bay Foreshore Master Plan 2012 for details.



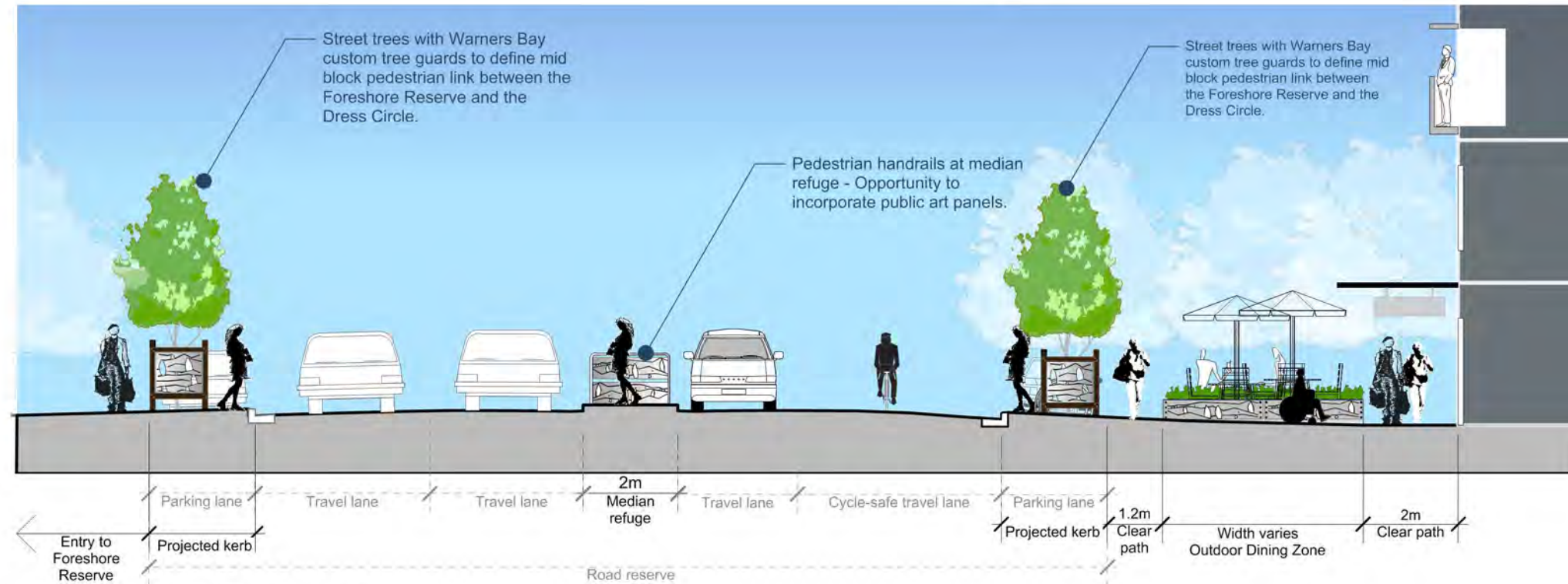
### Esplanade Concept Plan

This concept plan for The Esplanade has been prepared to show how the street may be developed over the life of this plan. The locations of elements on this plan are indicative and may change. Detailed design is required to determine final locations for retaining walls, ramps, stairs, trees, furniture etc.

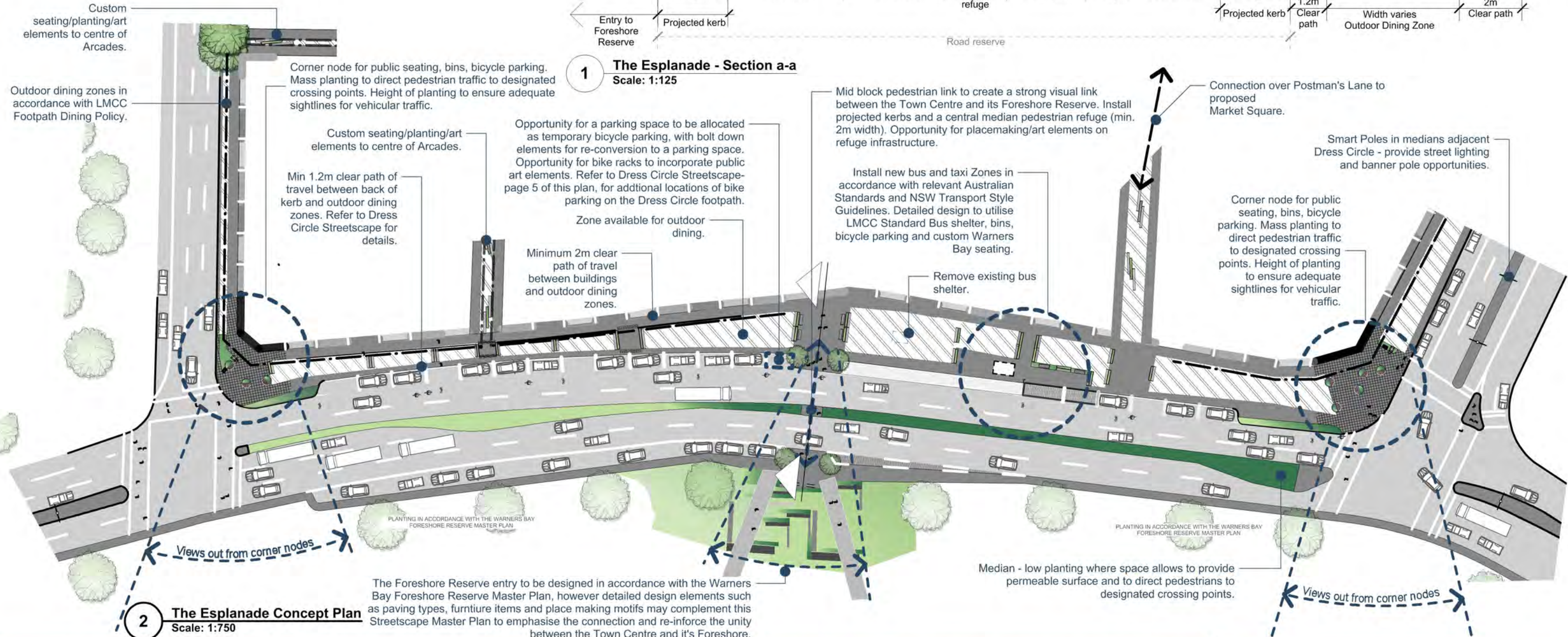
This concept incorporates the changed traffic conditions proposed in the *Warners Bay Town Centre Framework*. The street layouts and intersection treatments are based around these proposed traffic configurations.

#### PRINCIPLES

- Reduce visual obstruction / open views across to the foreshore and lake.
- Create a strong physical and visual connection between the Town Centre and its Foreshore.
- Create rest opportunities where views can be enjoyed
- Maximise pedestrian space and opportunities to activate The Esplanade streetscape.
- Promote alfresco dining with distinctive covered dining / seating areas.
- Improve compliance with disabled access standards
- Provide adequate cycle access and parking to the Esplanade and Foreshore Reserve.



**1 The Esplanade - Section a-a**  
Scale: 1:125





### Market Square Concept Plan

This concept plan for a Warners Bay Market Square has been prepared to show how this area may be developed over the life of this plan. The locations of elements on this plan are indicative and may change. Detailed design is required to determine final locations for retaining walls, ramps, stairs, trees, furniture etc.

Market Square would need to be undertaken as a major capital works project.

#### PRINCIPLES

- Provide generous level spaces for outdoor events, markets and community gathering.
- Utilise platforms, steps, and paths to create a multifunctional topography that can be used for circulation, passive recreation, public art and place-making.
- Incorporate public art, graphics, green walls, projections to activate vertical faces.
- Minimise or eliminate the need for handrails.
- Provide a linear access to physically and visually connect John St to the Bay Arcade.
- Arrange elements to maximise areas of summer shade and winter sun.



Opportunity for public art on steps and level changes



Seating platforms offer urban lounging opportunities.



Steps and accessible paths: the topography of circulation



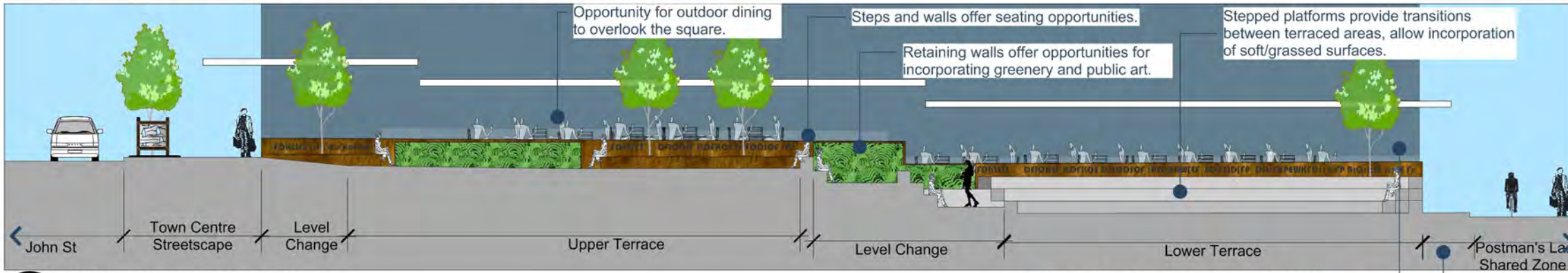
Example of linear axis for circulation and views; green walls and level terraces for passive recreation



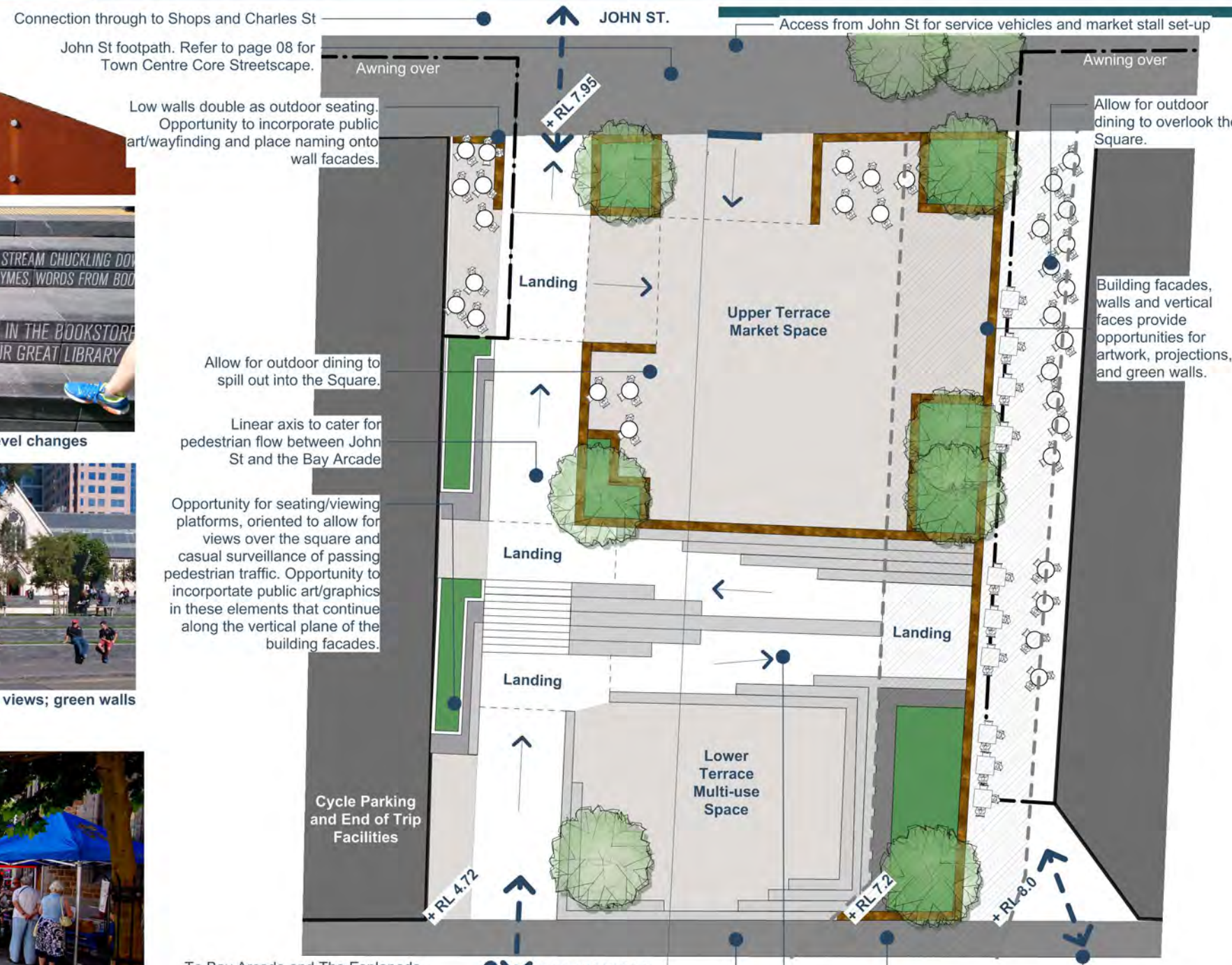
Example of seating platforms aligned with building facades



Level spaces for market stalls and events



**2 Section through Market Square**  
Scale: 1:200



**1 Market Square Concept Plan**  
Scale: 1:300

Existing section of Postmans Lane to be closed to traffic.

Level pedestrian access from Postman's Lane.

Utilise steps, platforms and accessible pathways to transition between changes in level to create multifunctional spaces that cater for circulation and passive recreation. Path gradients shallower than 1 in 21 to minimise the need for handrails.

Postman's lane footpath. Refer to Page 12 for details.

2m setback from Postmans Lane.



### LOCATION

- Along the key (primary and secondary) edges of the town centre as identified in the Warners Bay Town Centre Strategy.
- Wrapping the frontages of Development Block A (as identified in the Warners Bay Area Plan) along Lake St, The Esplanade and King St.

### OBJECTIVES

- To cater for a range of retail, commercial, business and hospitality uses.
- To provide a safe, accessible and vibrant destination that visually connects the Town Centre to it's Lake Foreshore.
- To create a well designed public domain with high quality finishes
- To encourages patronage of local businesses.
- To respond to the built form of the 'street wall' contained in the *Draft Warners Bay Town Centre Area Plan* development controls for Building Type A.

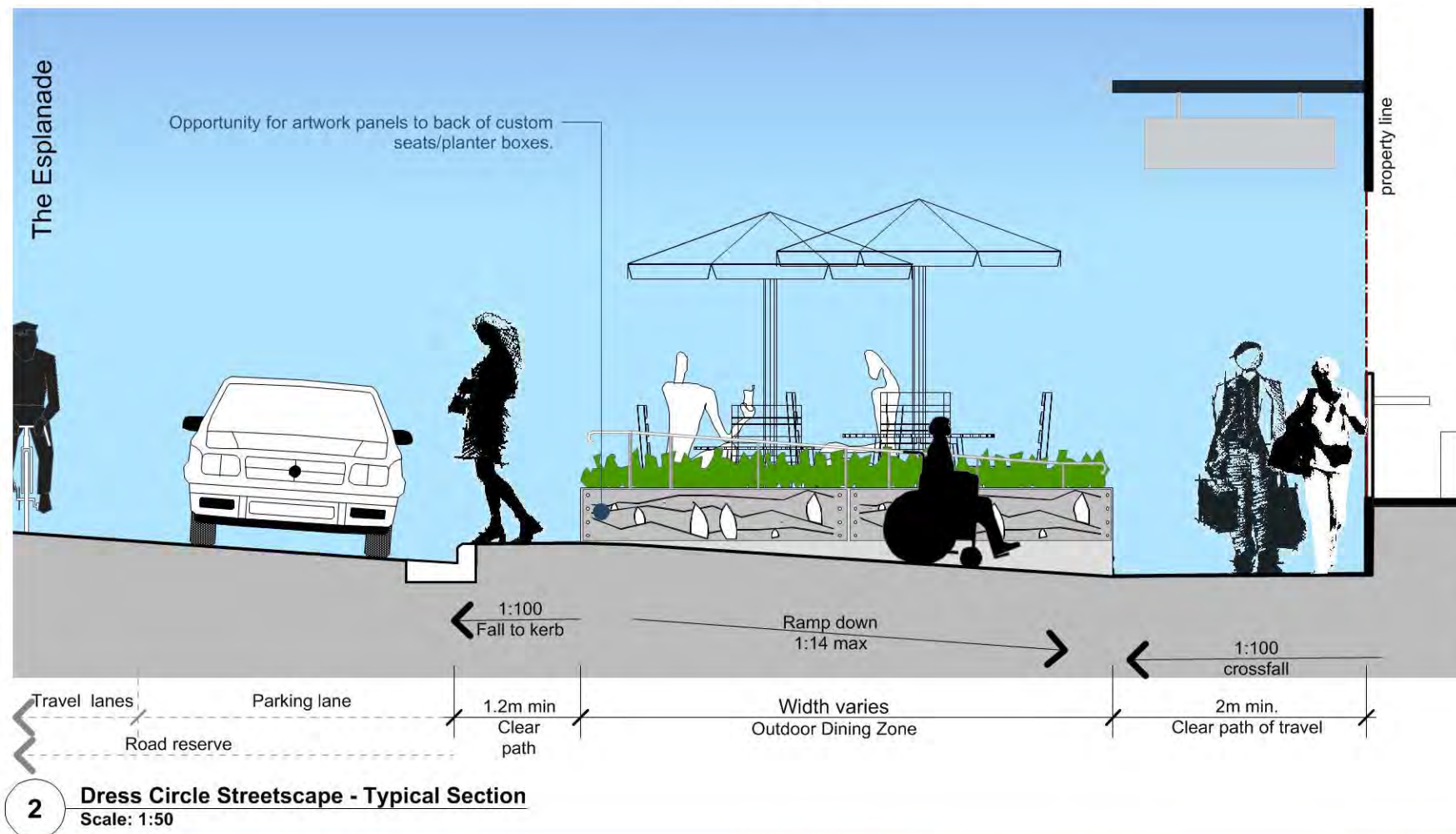
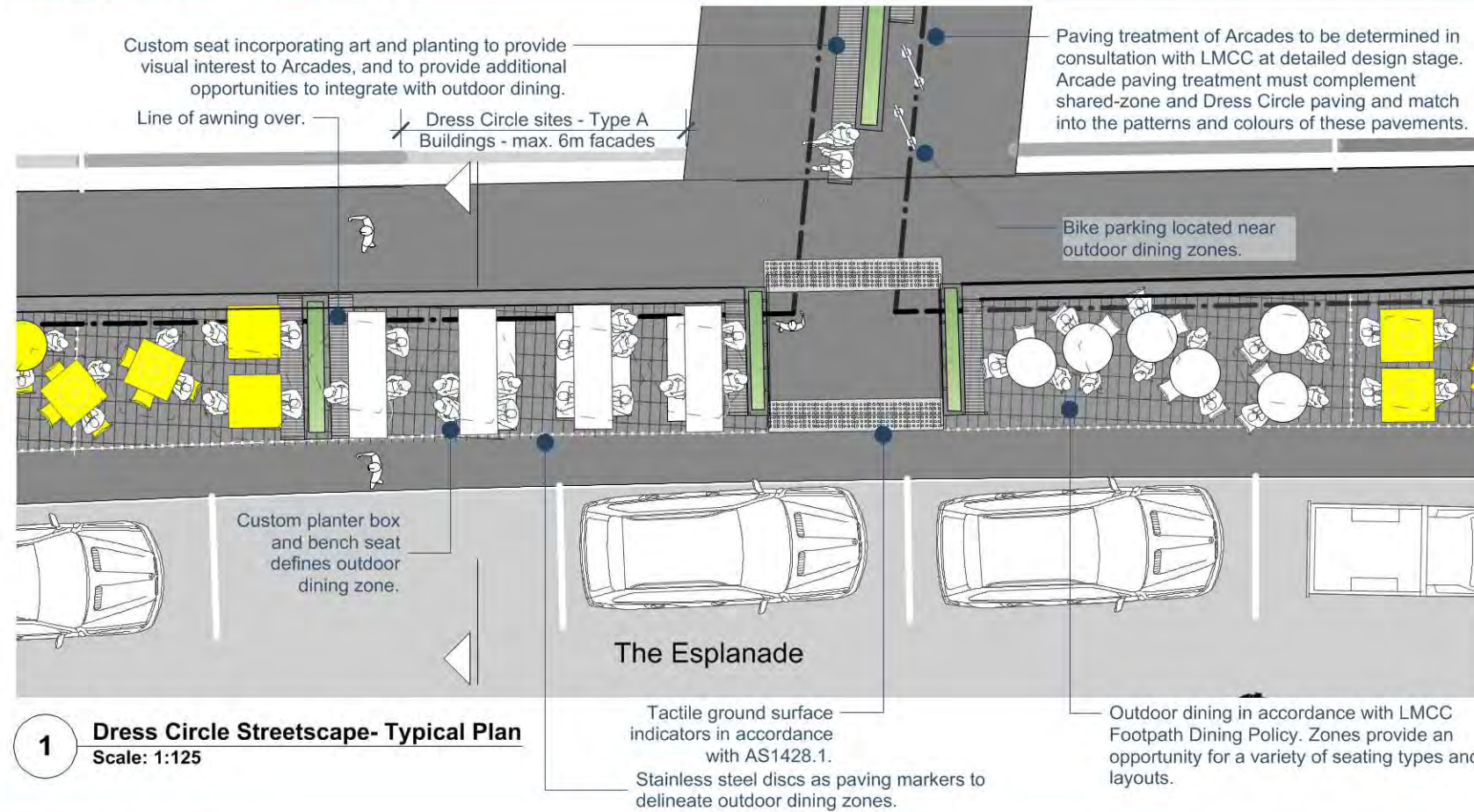
### EXISTING CONSTRAINTS

- Overhead power lines.
- Angle parking minimises footpath width.
- Footpath surface levels are lower than those on the adjacent road carriageway.
- Competing needs of vehicular through-traffic on The Esplanade and pedestrian connections between the town centre and it's lake foreshore.
- Roads & Maritime Services (RMS) requirements will be applicable for state managed roads - The Esplanade.

### DESCRIPTION

- Full width paving maximises pedestrian traffic movement along the street frontage.
- Outdoor dining platforms provide a raised dining opportunity clear of pedestrian and vehicle movement and improving views over the Esplanade to the lake foreshore beyond.
- Corner nodes provide opportunities for public seating, bike parking bubbler and other street furniture elements.

### Key



### MATERIALS AND FURNITURE SELECTIONS

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to *Warners Bay Streetscape Technical Guidelines* for **specification guidance on streetscape elements selections and pavements.**

#### Seating

**Locations:** Generally to define outdoor dining zones and create activity and gathering nodes at Dress Circle corner sites. Allow for a mix of custom and proprietary seating to provide a variety of seating types.

**Selections:** Custom planter and seat - Type 1 (CPS1); Custom planter and seat - Type 2 (CPS2) - Refer to Pages 12 and 13 of this plan.

#### Bike Parking

**Locations:** At key locations along cycle routes, building entries and near outdoor dining zones. Cluster in groups of two or three.

#### Drinking Fountains

**Locations:** At key locations along cycle routes and at activity and gathering nodes.

#### Waste receptacles- recycling and rubbish bins

**Locations:** At bus stops, taxi stands, along key pedestrian routes and at activity and gathering nodes. Ensure enclosures are oriented to open toward the building line.

#### STREET TREES

**Locations:** Refer to the *Street Tree Master Plan* on Page 14 of this plan.

**Tree Guards:** Warners Bay custom timber tree guard with stainless steel decorative panels. Refer to Page 15 of this plan.

#### MASS PLANTING

**Locations:** Adjacent furniture elements where awnings prevent street tree plantings.

**Species:** Native strap-leaved plants, native grasses or groundcovers. Species selection to be determined at detailed design stage. Refer to *Warners Bay Streetscape Technical Guidelines* for **planting, mulching and maintenance requirements.**

#### PAVEMENT

Paving arrangement and materials to be determined at detailed design stage to suit site specific conditions and design response.

**Pavement design should take cues from the Warner's Bay Foreshore area to establish a visual link between the town centre and it's foreshore.**

#### Pavement Markers - to delineate outdoor dining licences

Stainless steel markers

#### Tactile Paving

Integrated TGS1 paving units. Set-out installation to AS1428.4.

#### Permeable Paving

Locate to tree pits as directed in the *Warners Bay Streetscape Technical Guidelines.*

#### Kerb ramps

Concrete, colour to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines.*



## LOCATION

• King St, between The Esplanade and John St.

## OBJECTIVES

- To cater for a range of retail, commercial, business and hospitality uses.
- To provide a safe, accessible and vibrant destination that visually connects the Town Centre to its Lake Foreshore.
- To create a well designed public domain with high quality finishes
- To encourage patronage of local businesses.
- To respond to the built form of the 'street wall' contained in the *Warners Bay Town Centre Area Plan* development controls for Building Type A.
- To provide a dedicated route for cyclists to access the Warners Bay Foreshore from King St that is safe for cyclists, and compatible with pedestrian and vehicle circulation.

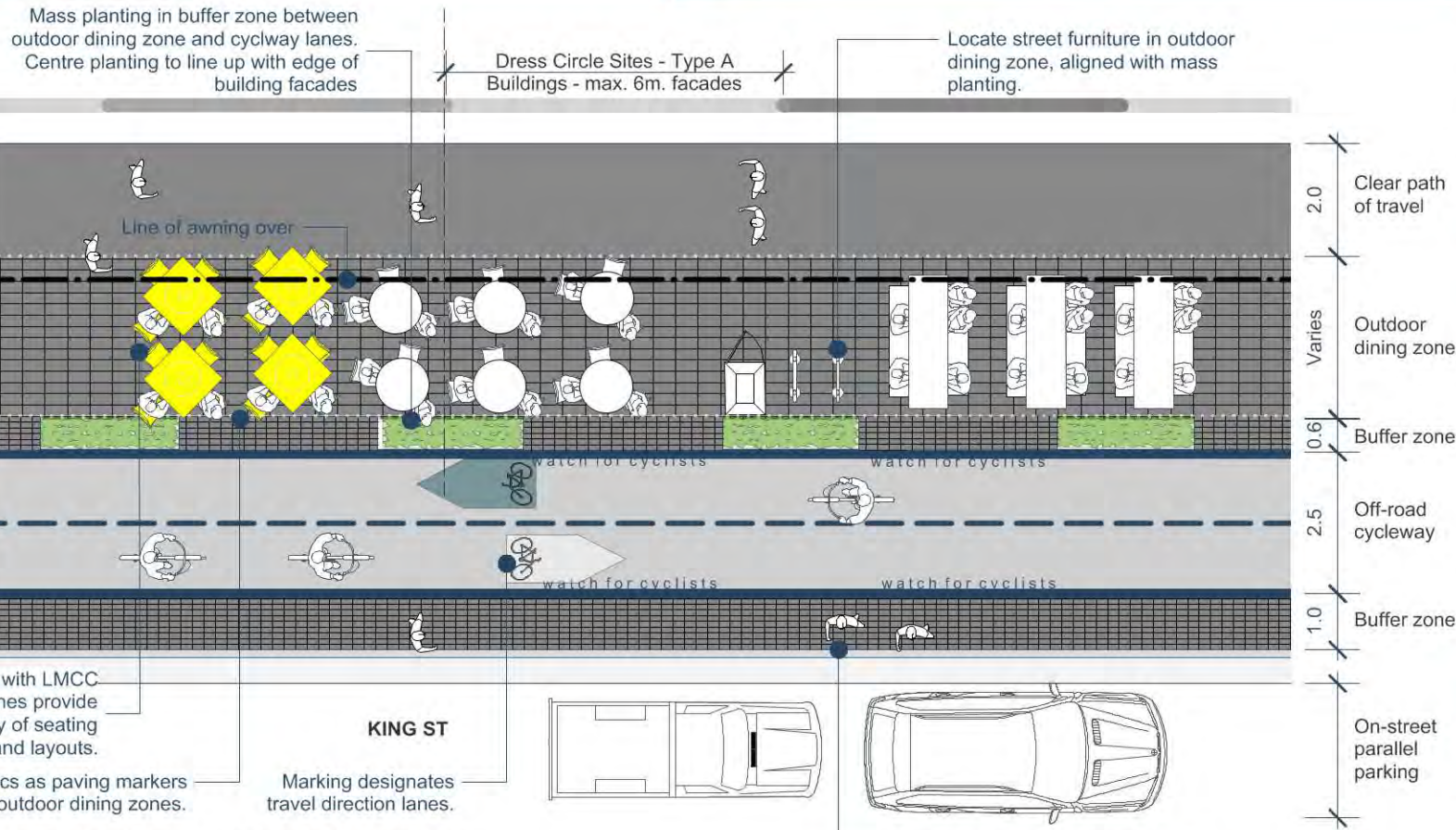
## EXISTING CONSTRAINTS

- Overhead power lines.
- Angle parking minimises footpath width.

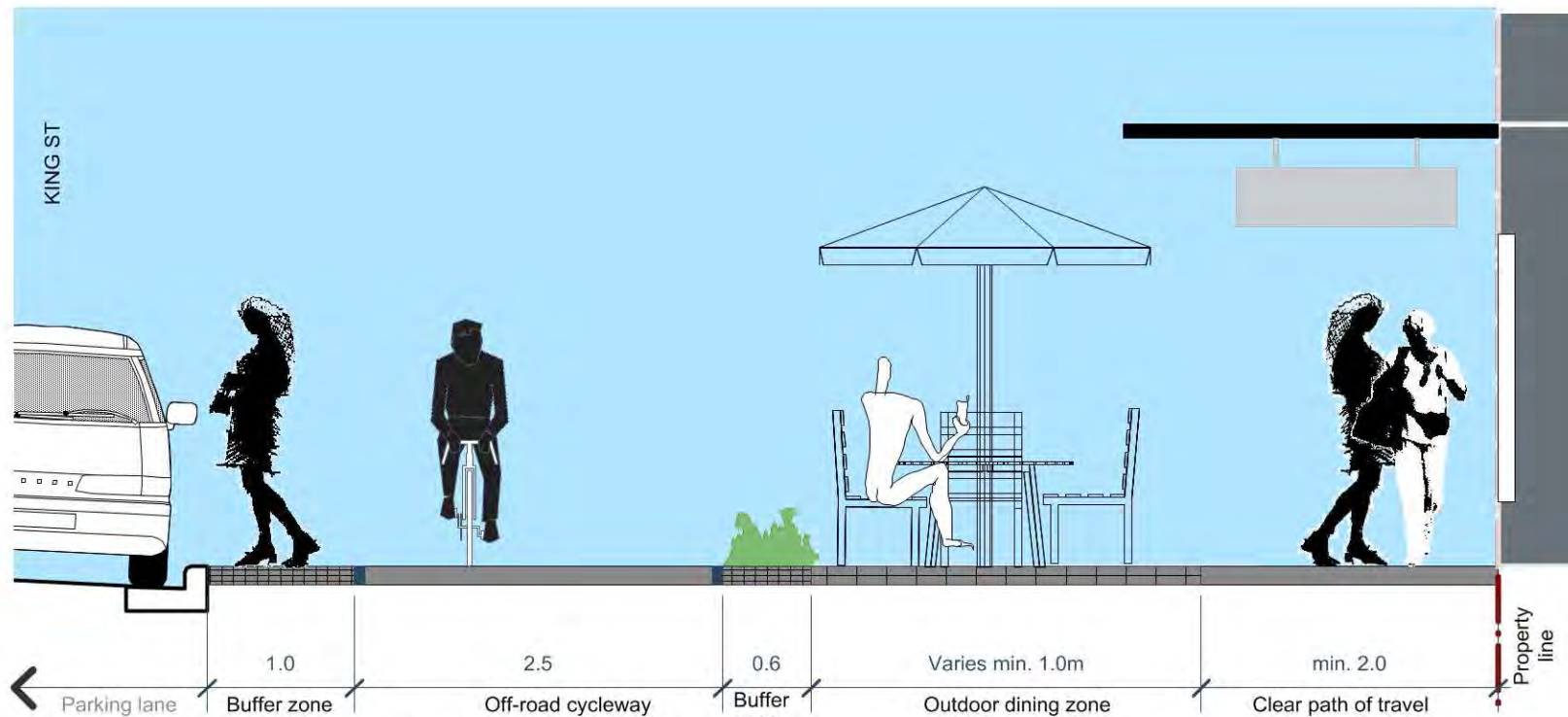
## DESCRIPTION

- Re-align existing angle parking to parallel parking configuration to maximise footpath width.
- Clear path of travel for pedestrians adjacent building facades.
- Outdoor dining areas provide a dining opportunity clear of pedestrian and cyclist movement.
- A buffer zone between the back of kerb and cycleway to provide clearance from opening doors. Buffer zone delineated by feature paving to differentiate from the cycle lanes.
- A buffer zone between the outdoor dining spaces and cycleway to provide a clear separation and minimise conflicts between cyclist and pedestrian movement. Buff zone comprises a mix of feature paving and planting to differentiate from the cycle lanes.

## Key



**1 Dress Circle with cycleway- Typical Plan**  
Scale: 1:125



**2 Dress Circle with cycleway- Typical Section**  
Scale: 1:50

## MATERIALS AND FURNITURE SELECTIONS

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to *Warners Bay Streetscape Technical Guidelines* for specification guidance on streetscape elements selections and pavements.

### Seating

**Locations:** At key locations identified by LMCC at detailed design stage.

### Bike Parking

**Locations:** At key locations along cycle routes and at activity and gathering nodes.

### Drinking Fountains

**Locations:** At key locations along cycle routes and at activity and gathering nodes.

### Waste receptacles- recycling and rubbish bins

**Locations:** At key locations along cycle routes and at activity and gathering nodes. Ensure enclosures are oriented to open toward the building line.

## MASS PLANTING

**Locations:** Adjacent furniture elements where awnings prevent street tree plantings.

**Species:** Native strap-leaved plants, native grasses or groundcovers. Species selection to be determined at detailed design stage. Refer to *Warners Bay Streetscape Technical Guidelines* for planting, mulching and maintenance requirements.

## PAVEMENT

Paving arrangement and materials to be determined at detailed design stage to suit site specific conditions and design response. Pavement design should take cues from the *Warners Bay Foreshore area* to establish a visual link between the town centre and its foreshore.

### Pavement Markers - to delineate outdoor dining licences

Stainless steel markers

### Tactile Paving

Integrated TGS1 paving units. Set-out installation to AS1428.4.

### Permeable Paving

Locate to tree pits as directed in the *Warners Bay Streetscape Technical Guidelines*.

### Kerb ramps

Concrete, colour to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines*.



## LOCATION

- Adjacent Lake Macquarie Local Environment Plan B2-Local Centre Zones - the key business zones of the Town Centre.
- To correspond with Building Types B - F as identified in the *Warners Bay Town Centre Area Plan*.

## OBJECTIVES

- To respond to the built form of the 'street wall' contained in the *Warners Bay Town Centre Area Plan* development controls for Building Types B - F.
- To create public domain spaces that facilitate the 'active street frontages' contained in the *Warners Bay Town Centre Area Plan* development controls.
- To cater for a range of retail, commercial, and business.
- To maximise accessibility and use of public transport, walking and cycling.
- To create a sense of place and emphasise visual and physical connections to the lake and recreational spaces.

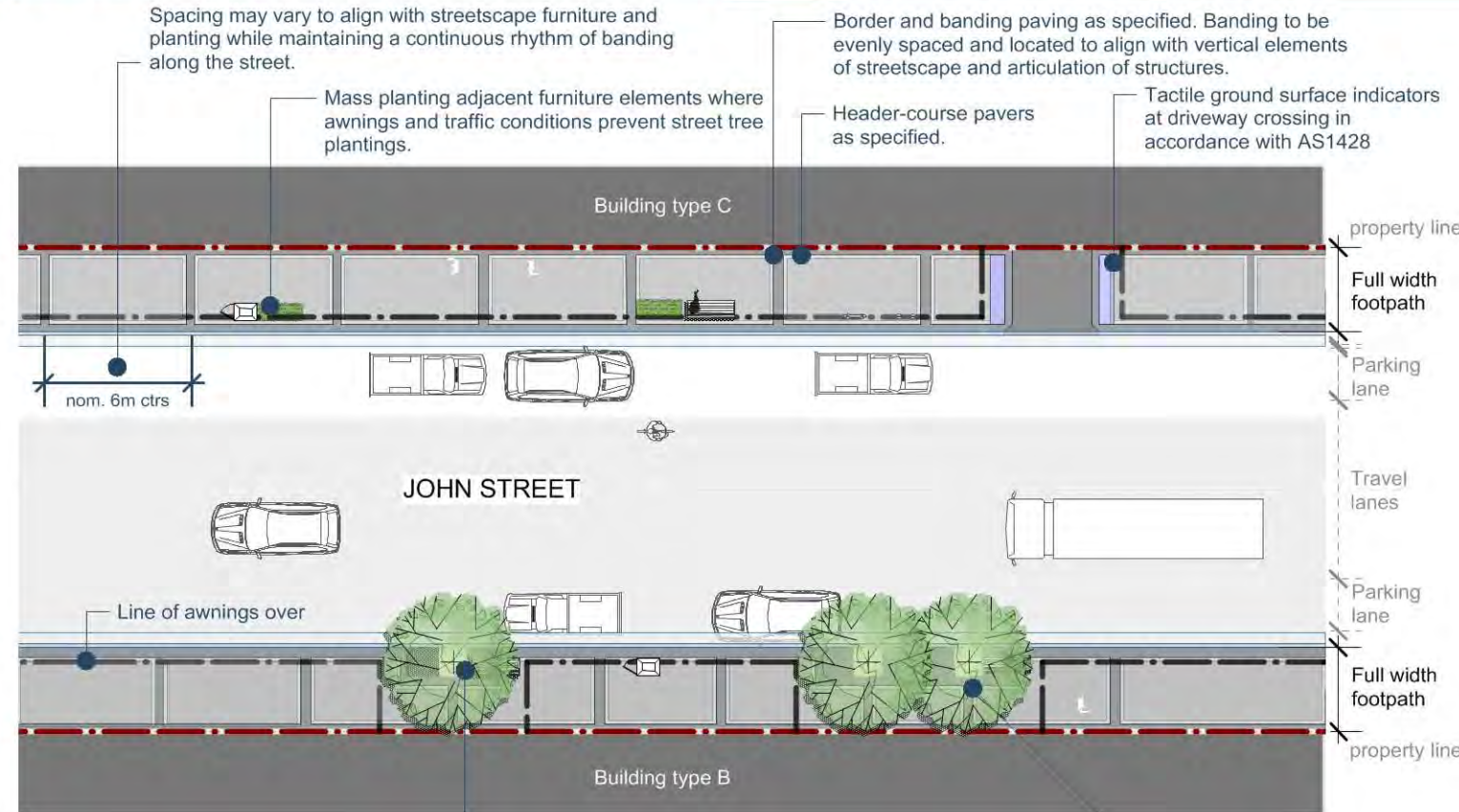
## EXISTING CONSTRAINTS

- Overhead powerlines
- Underground services.
- Various existing road reserve widths

## DESCRIPTION

- Full width pavement from property boundaries to back of kerb - maximises opportunities for a variety of uses.
- Small street trees to back of kerb where space allows, or within the parking lane pavement, to provide shade, soften the hard surfaces and link to surrounding natural areas.
- Mass planting adjacent furniture elements where overhead awnings prevent street tree planting.

## Key

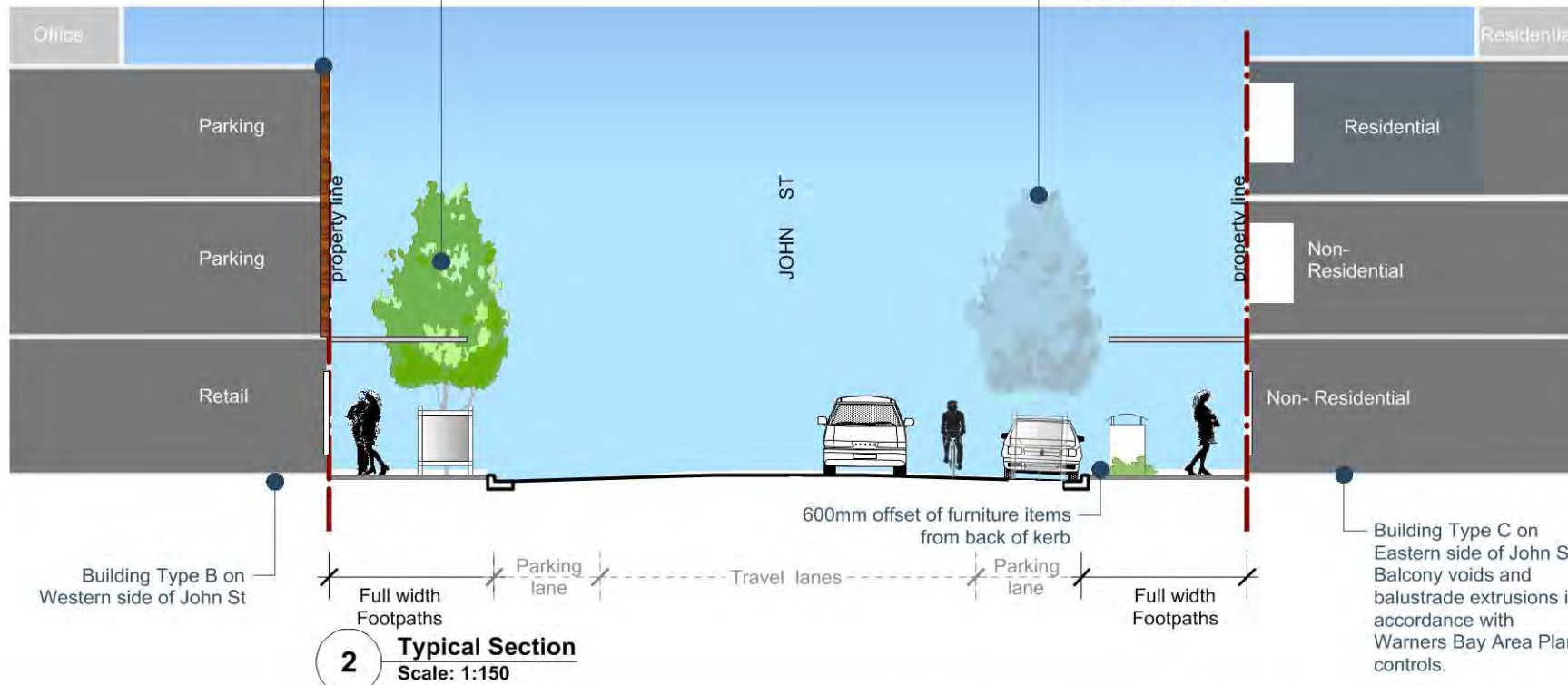


**1 Typical Plan**  
Scale: 1:300

The Warners Bay Town Centre Area Plan Building Type B controls require the incorporation of louvres, perforated sheet and public art overlays to car-park level facades to enliven the street elevation and contribute to interest on the vertical plane of the streetscape.

Street tree planting in pavement, with custom Warners Bay tree guard. Refer to LMCC Landscape Standard Detail LSD\_PLA\_07 for installation details.

Street tree planting in the parking lane may be considered for Town Centre Core streetscapes where continuous awnings prevent tree planting on footpaths, subject to traffic conditions and available parking provision. Refer to LMCC Landscape Standard Detail LSD\_PLA\_03 for installation details.



**2 Typical Section**  
Scale: 1:150

## MATERIALS AND FURNITURE SELECTIONS

### FURNITURE

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to *Warners Bay Streetscape Technical Guidelines* for **specification guidance on streetscape element selections and pavements**.

#### Bench Seats

**Locations:** At key locations identified by LMCC at detailed design stage.

#### Bike Rail

**Locations:** At key locations along cycle routes, building entries and near outdoor dining zones. Cluster in groups of two or three.

#### Drinking Fountains

**Locations:** At key locations along cycle routes and at activity and gathering nodes.

#### Waste receptacles- recycling and rubbish bins

**Locations:** At bus stops, taxi stands, along key pedestrian routes and at activity and gathering nodes.

### STREET TREES

**Locations:** Refer to the *Street Tree Master Plan* on Page 14 of this plan.

**Tree Guards:** Warners Bay custom timber tree guard with stainless steel decorative panels. Refer to Page 15 of this plan.

### MASS PLANTING

**Locations:** Adjacent furniture elements where awnings prevent street tree plantings.

**Species:** Native strap-leaved plants, native grasses or groundcovers. Species selection to be determined at detailed design stage. Refer to *Warners Bay Streetscape Technical Guidelines* for **planting, mulching and maintenance requirements**.

### PAVEMENT

#### Main Body Pavement

Concrete with exposed aggregate finish in accordance with the *Warners Bay Streetscape Technical Guidelines*.

#### Header-course

Pale grey/silver colour pavers with blue/silver/black aggregate exposed through a honed finish. Lay in a landscape orientation as a single row between the border/banding and main body paving as shown on the typical plan this page.

#### Borders and Banding

Pavers in a dark Charcoal colour with light grey aggregate exposed through a honed finish. Lay as a single row of pavers against the back of kerb and building edge to create a pavement border, separated by a single row of pavers as banding as shown on the typical plan this page.

#### Pavement Markers

Locate to delineate outdoor dining licences  
Stainless steel markers

#### Tactile Paving

Integrated TGS1 paving units. Set-out installation to AS1428.4.

#### Permeable Paving

Locate to tree pits as directed in the *Warners Bay Streetscape Technical Guidelines*.

#### Kerb ramps

Concrete, colour to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines*.



## LOCATION

- Along minor and supporting edges of the town centre as identified in the *Warners Bay Town Centre Area Plan*.
- To correspond with Building Type G as identified in the *Warners Bay Town Centre Area Plan*.
- Addresses the interface between the business and recreational spaces of the Town Centre.
- Addresses the interface between the business and residential spaces of the Town Centre.

## OBJECTIVES

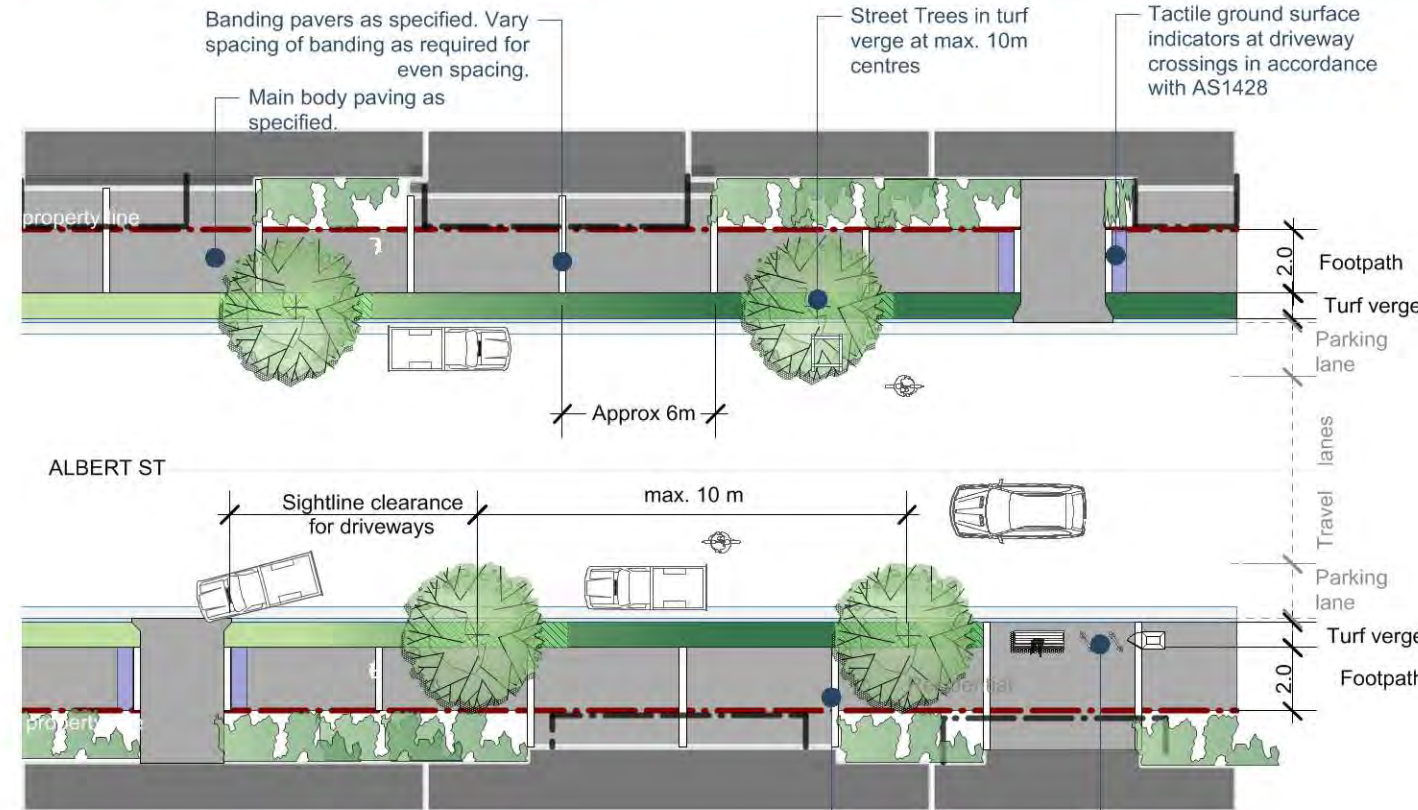
- To pedestrian access and circulation to and within the town centre.
- To accommodate commercial and medium density residential uses.
- To provide a transition between the town centre core streetscapes, residential streetscapes and recreational areas.
- To respond to the building form and setback controls contained in the *Warners Bay Town Centre Area Plan* for Building Type G.

## EXISTING CONSTRAINTS

- Overhead power lines.
- Various existing road reserve widths.
- Underground services

## DESCRIPTION

- Minimum 2m wide pavements stretching from the property boundary towards the verge to accommodate a mix of retail, commercial and residential uses.
- Turf verges with street tree planting where desirable and feasible.

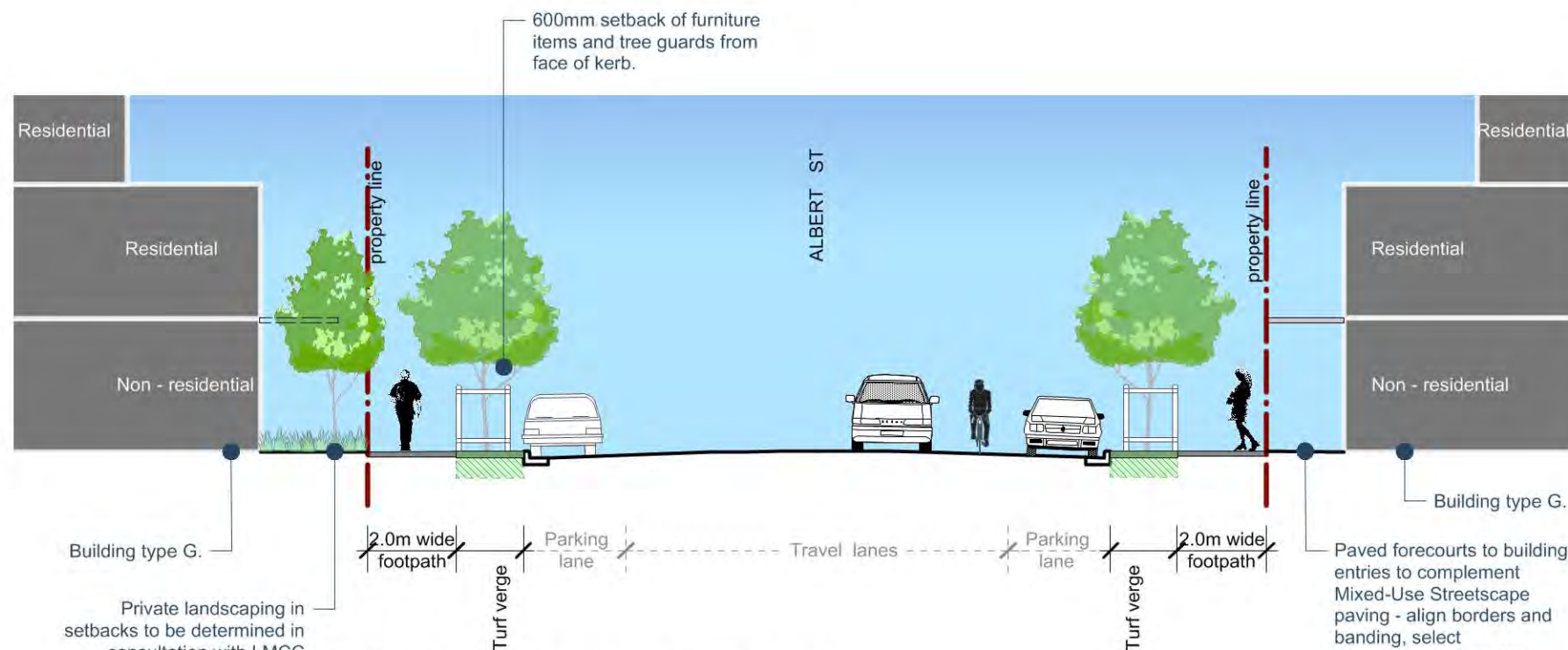


**1 Mixed Use Streetscape - Typical Plan**

Scale: 1:300

Align pavement banding with building articulation and extents of private forecourt entry paving. Where possible, consider extending banding into private forecourt areas to create a unifying link between public and private paving treatments.

## Key



**2 Mixed Use Streetscape - Typical Section**

Scale: 1:150

## MATERIALS AND FURNITURE SELECTIONS

### FURNITURE

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to *Warners Bay Streetscape Technical Guidelines* for specification guidance on streetscape element selections and pavements.

### Bench Seats

**Locations:** At key locations identified by LMCC at detailed design stage.

### Bike Rail

**Locations:** At key locations along cycle routes, building entries and near outdoor dining zones. Cluster in groups of two or three.

### Waste receptacles- recycling and rubbish bins

**Locations:** At bus stops, taxi stands, along key pedestrian routes and at activity and gathering nodes.

### STREET TREES

**Species selection:** Refer to the Street Tree Master Plan on Page 14 of this plan.

**Tree Pit construction:** Refer to LMCC Landscape Standard Drawings LSD-PLA-01 Tree Pit in Turf (with footpath).

**Tree Guards:** Warners Bay custom Tree Guard- NO decorative panels.

### PAVEMENT

#### Main Body Paving

Natural grey concrete- broom finish. No trowel edges.

#### Banding

A single row of unit pavers. Paver type as specified in the *Warners Bay Streetscape Technical Guidelines*. Banding at nominal 6m ctrs but may be varied to align with vertical building elements, building entries, street furniture elements while maintaining a continuous rhythm of banding along the street.

#### Tactile Paving

Integrated TGS paving units. Set-out installation to AS1428.4.

#### Kerb ramps

Concrete, coloured to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines*.



## LOCATION

- Along Lake St, King St, and Charles St as identified on page 3 of this plan.

## OBJECTIVES

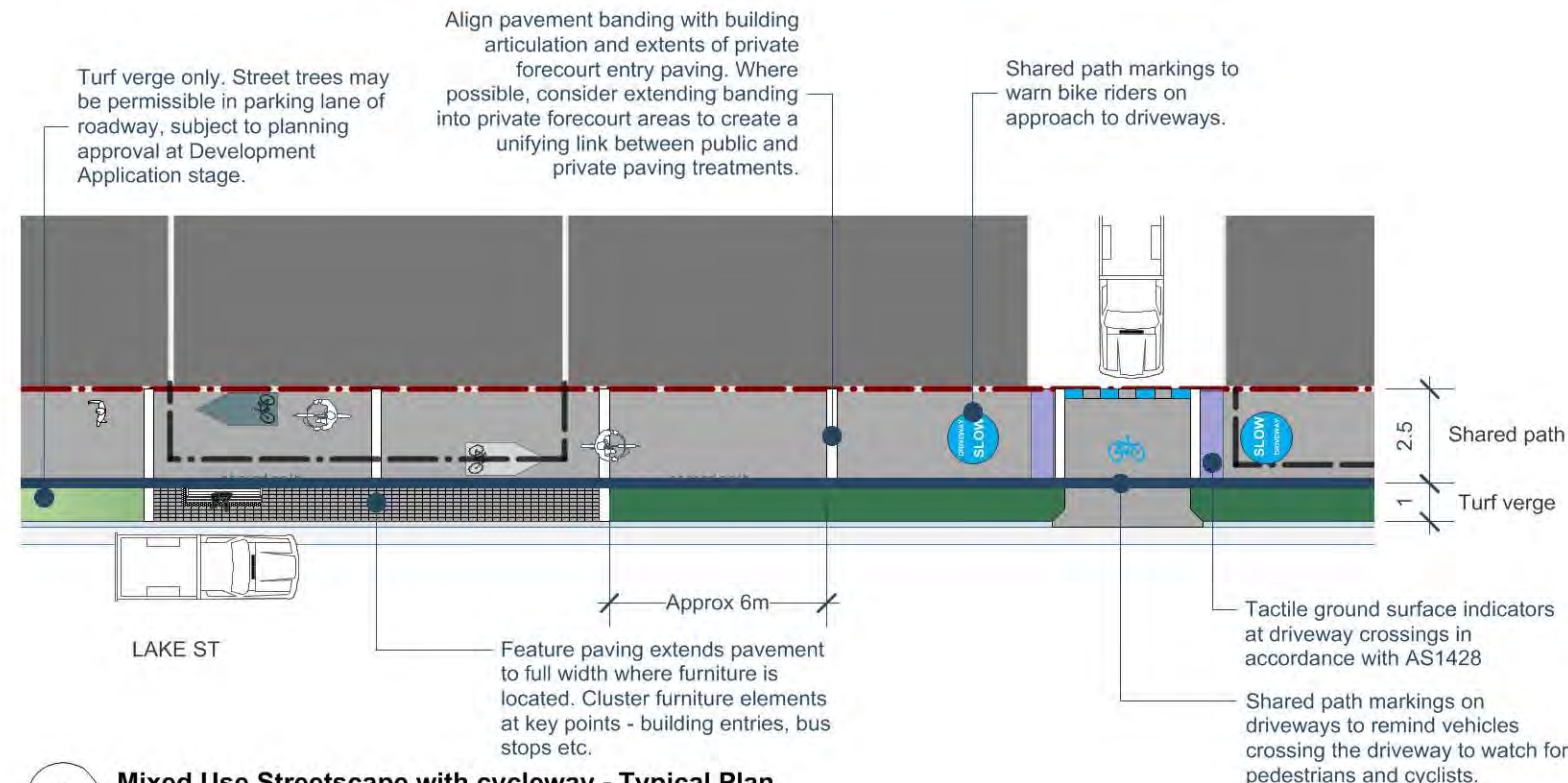
- To improve pedestrian and cyclist circulation to and within the town centre.
- To provide off-road cyclist access through the town centre to the Warners Bay Foreshore Reserve.

## EXISTING CONSTRAINTS

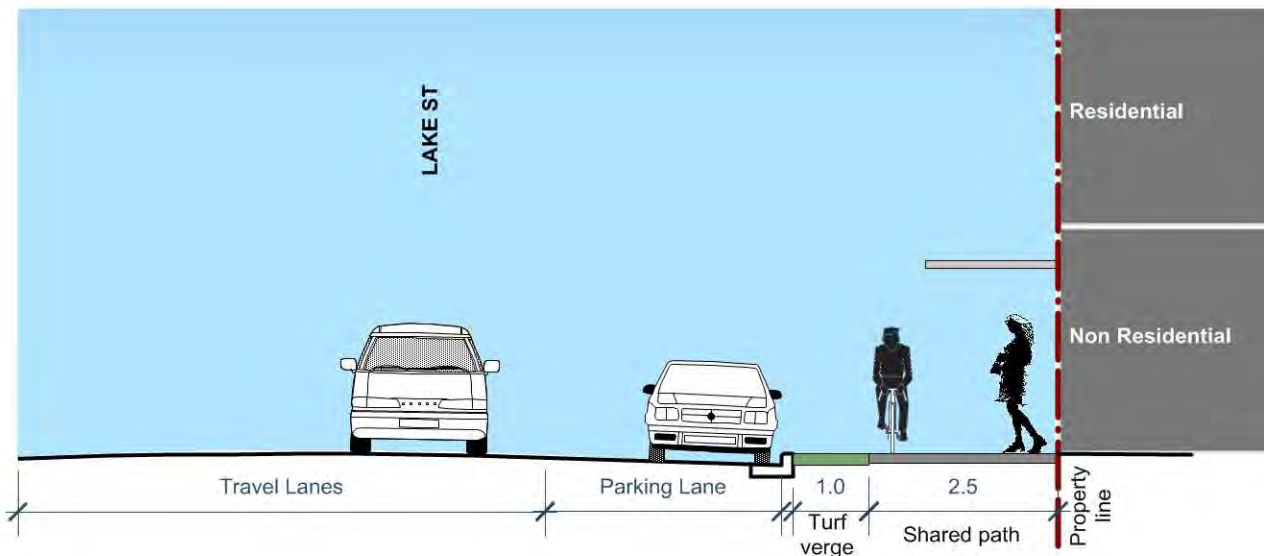
- Overhead power lines.
- Various existing road reserve widths.
- Underground services
- Driveway crossings

## DESCRIPTION

- Minimum 2.5m wide pavements stretching from the property boundary towards the verge in accordance with LMCC standard width for shared paths.
- Shared path markings to highlight pedestrian and cyclist usage of the pathways, and to designate direction of cyclist travel.
- Shared path markings to provide warnings to pedestrians, cyclists and vehicles at potential points of conflict, ie, at driveway crossings.
- At key points, building entries, bus stops, etc. the pavement may be extended to the back of kerb to provide street furniture elements as determined appropriate during the development application process.
- Turf verges only. Subject to the Development Application process, street trees may be proposed for inclusion within the parking lane of the roadway, or within landscape zones of private setback areas.



**1 Mixed Use Streetscape with cycleway - Typical Plan**  
Scale: 1:200



**2 Mixed Use Streetscape with cycleway - Typical Section**  
Scale: 1:100

## MATERIALS AND FURNITURE SELECTIONS

### FURNITURE

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to *Warners Bay Streetscape Technical Guidelines* for specification guidance on streetscape element selections and pavements.

### Bench Seats

**Locations:** At key locations identified by LMCC at detailed design stage.

### Bike Rail

**Locations:** At key locations along cycle routes, building entries and near outdoor dining zones. Cluster in groups of two or three.

### Waste receptacles- recycling and rubbish bins

**Locations:** At bus stops, taxi stands, along key pedestrian routes and at activity and gathering nodes.

### STREET TREES

**Species selection:** Refer to the *Street Tree Master Plan* on Page 14 of this plan.

**Tree Pit construction:** Refer to LMCC Landscape Standard Drawing LSD-PLA-03 Tree Pit in road (flush, on-street parallel parking)

**Tree Guards:** Warners Bay custom Tree Guard- NO decorative panels.

### PAVEMENT

#### Main Body Paving

Natural grey concrete- broom finish. No trowel edges.

#### Feature paving

**Locations:** Where pavement is extended to back of kerb, at street furniture locations.

**Type:** Concrete with an exposed aggregate finish as specified in the *Warners Bay Streetscape Technical Guidelines*.

#### Banding

A single row of unit pavers. Paver type as specified in the *Warners Bay Streetscape Technical Guidelines*. Banding at nominal 6m ctrs but may be varied to align with vertical building elements, building entries, street furniture elements while maintaining a continuous rhythm of banding along the street.

#### Tactile Paving

Integrated TGS1 paving units. Set-out installation to AS1428.4.

#### Kerb ramps

Concrete, coloured to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines*.

## Key





### LOCATION

• Adjacent land zoned R3- Medium Density Residential under Lake Macquarie Local Environment Plan 2014.

### OBJECTIVES

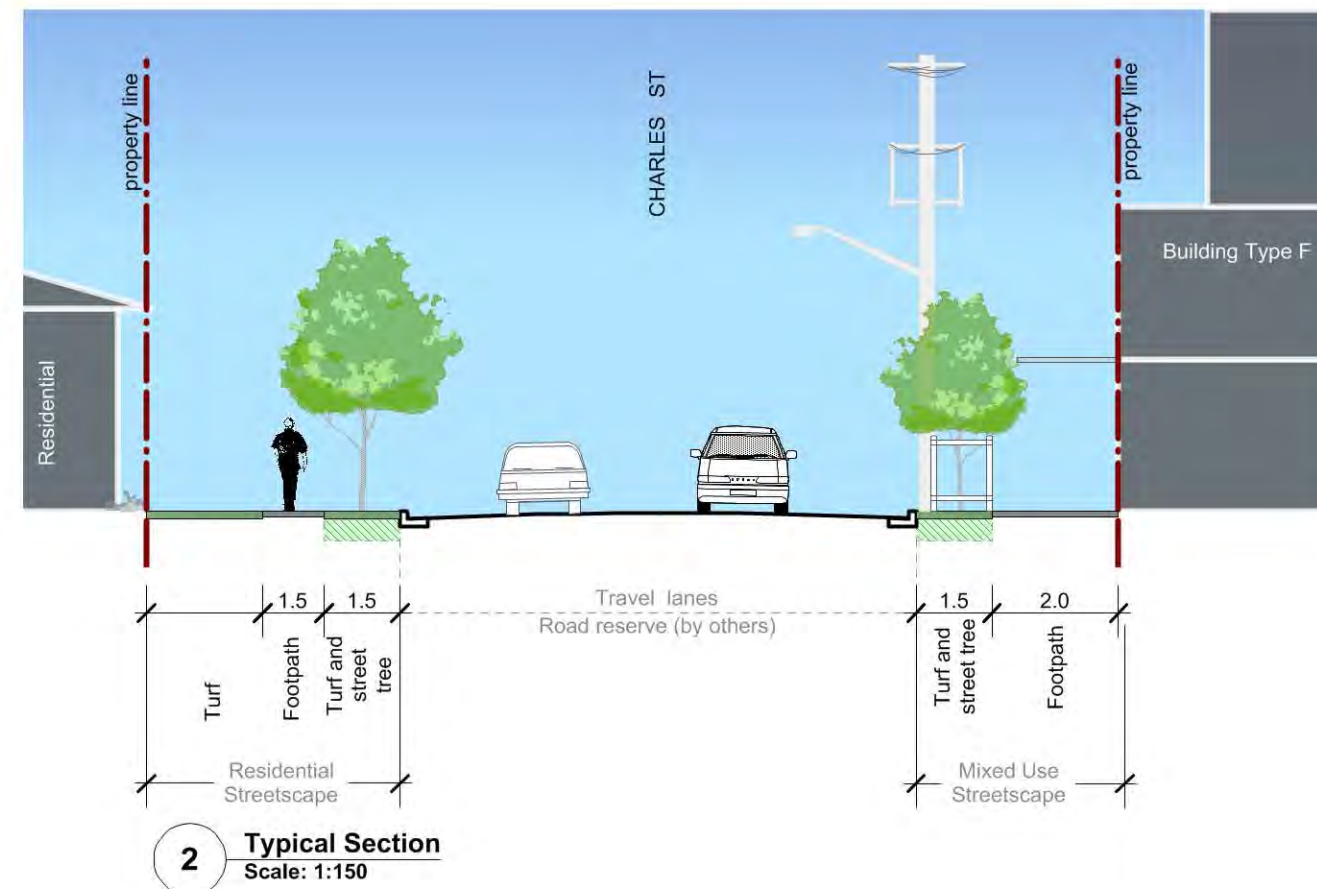
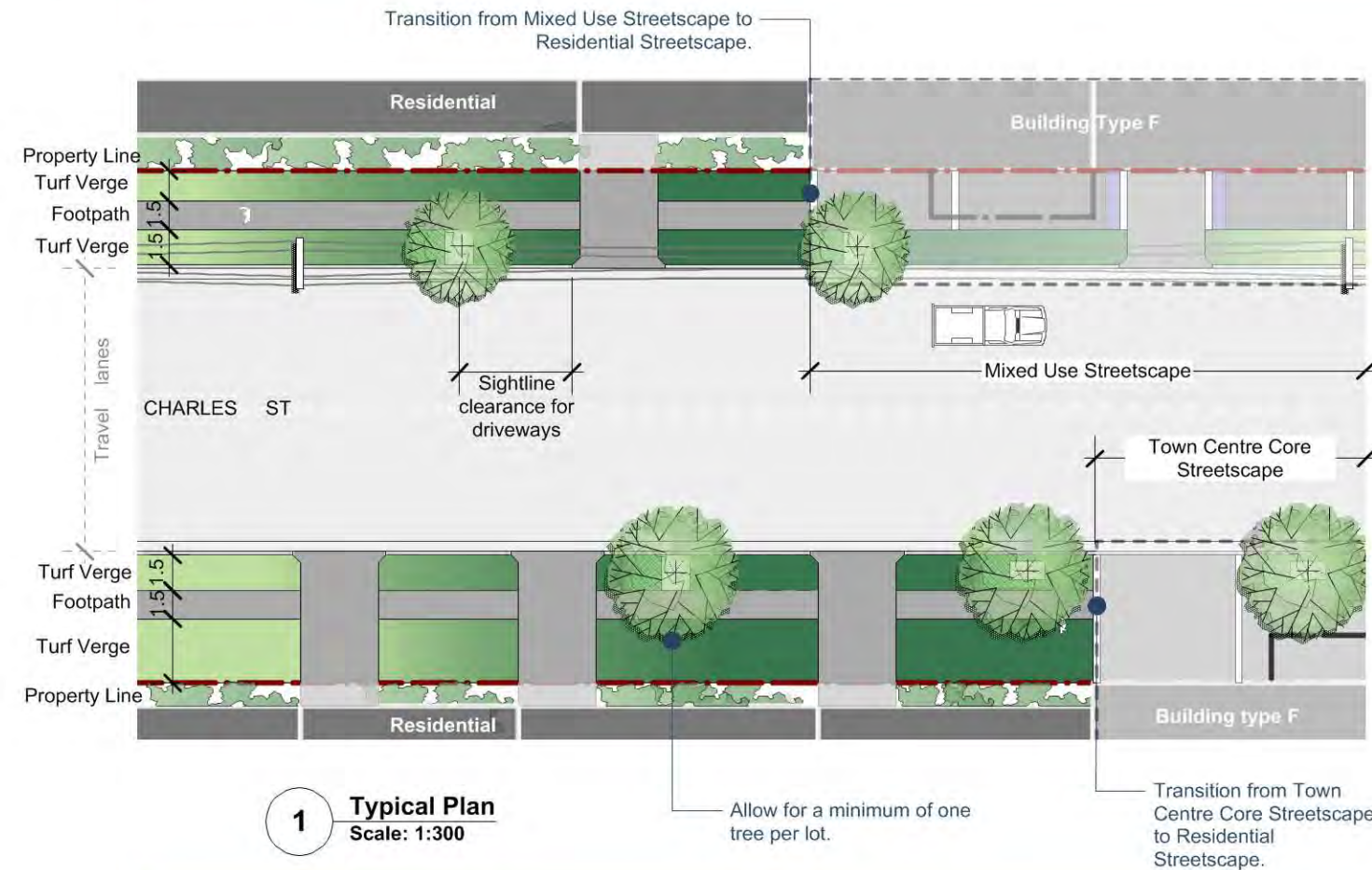
- To cater for local neighbourhood pedestrian requirements.
- To provide safe and accessible residential environments.
- To provide pedestrian linkages to recreational, business and core town centre areas.

### EXISTING CONSTRAINTS

- Overhead power lines.
- Irregular placement of poles and street signs

### DESCRIPTION

- Minimum 1.5m wide concrete footpaths to accommodate pedestrian volumes generated by medium density development.
- Street trees in a turf verge (minimum of one tree per lot unless LMCC Council Planning officers are satisfied that street tree installation is not feasible.)



### MATERIALS AND FURNITURE SELECTIONS

The following provide general guidelines for the arrangement of elements. Specific locations for items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments.

### FURNITURE

Not applicable on residential streetscapes.

### STREET TREES

Species selection: Refer to the *Street Tree Master Plan* on Page 14 of this plan.

Tree Pit construction: Refer to LMCC Landscape Standard Drawing LSD-PLA-01 Tree pit in turf (with footpath)

Tree Guards: Not applicable on Residential streetscapes.

### PAVEMENT

#### Footpath

Type: Concrete pavement.

#### Kerb Ramps

Concrete, coloured to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines*.

### Key





## DESCRIPTION

- Applicable to Postman's Lane, providing rear access to Dress Circle Buildings and John St car park development.
- Provides pedestrian links between The Esplanade, the Arcades, and John St.
- A low speed shared zones.

## OBJECTIVES

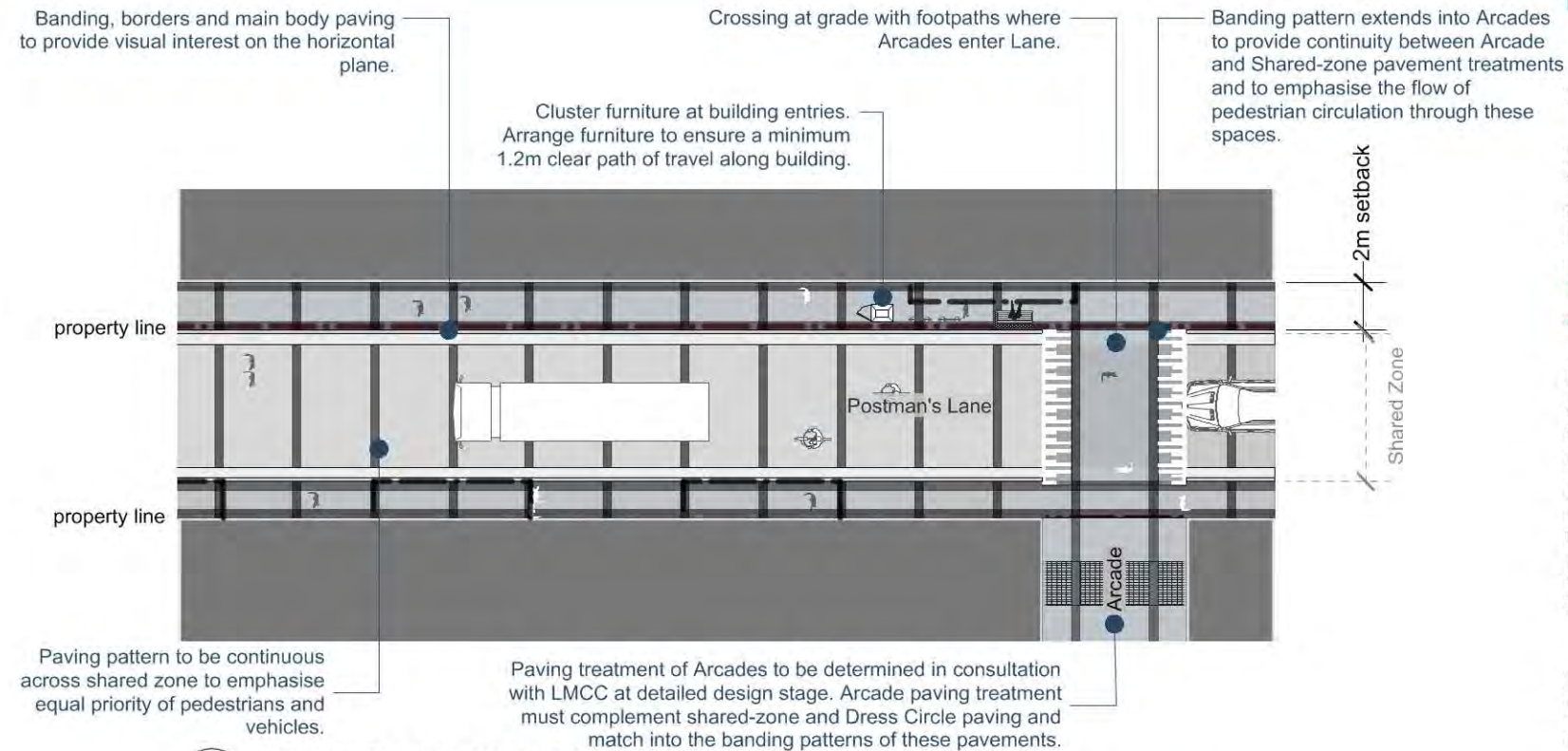
- To provide a safe and accessible thoroughfare for pedestrians and vehicles.
- To provide pleasant pedestrian environments for people to work and live.
- To respond to the built form of the 'street wall' contained in the Warners Bay Town Centre Area Plan development controls for Building Types A - B.

## EXISTING CONSTRAINTS

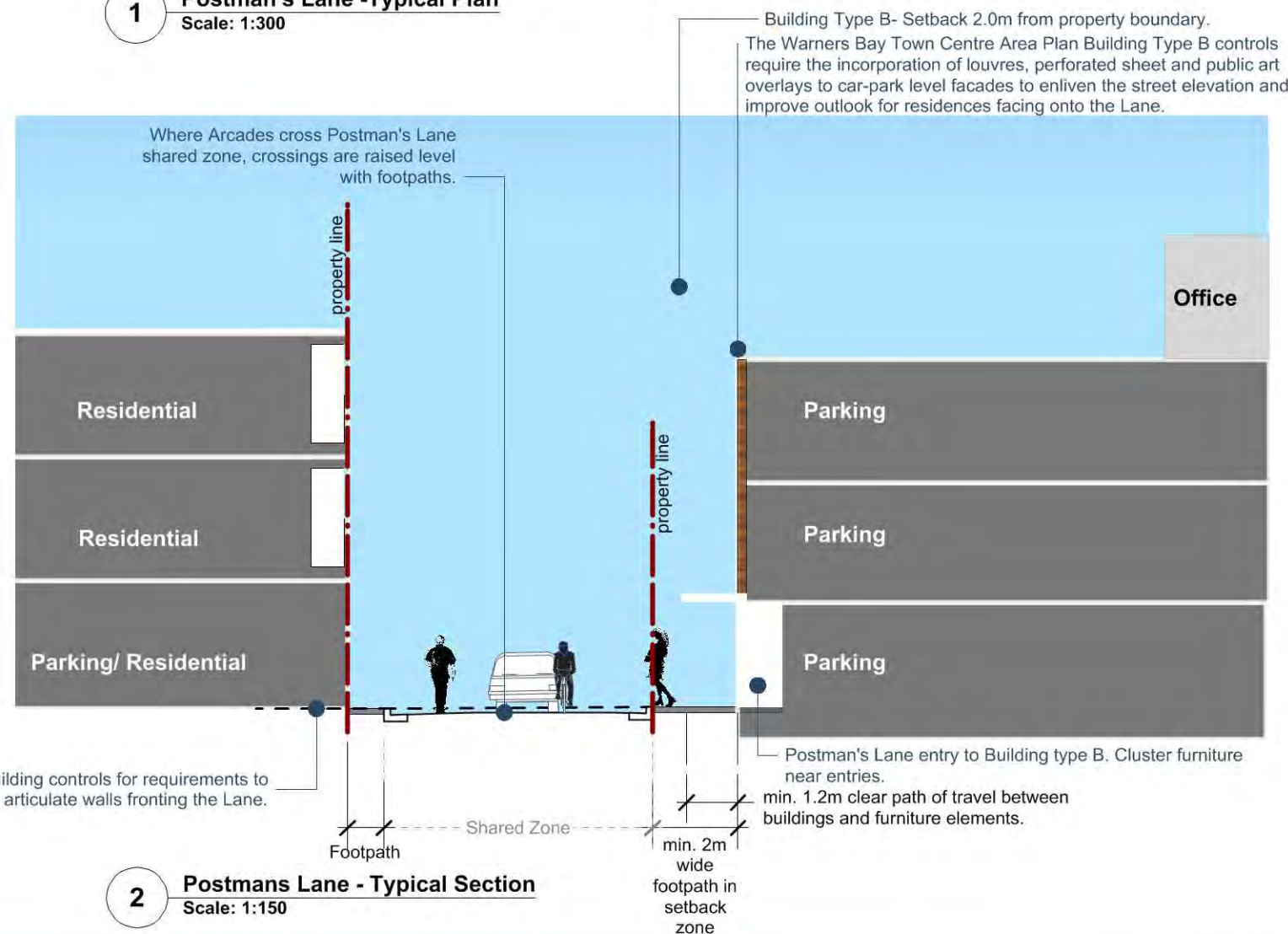
- Narrow lanes mostly without footpaths.
- Existing footpaths have varied surfaces, widths and grades.
- Generally lacking building frontage.
- Multiple vehicle access points onto the lane.

## DESIGN RESPONSE

- Create a shared zone environment, with continuity of pavement treatment to provide visual interest on the horizontal plane and to emphasise that pedestrians and vehicles have equal priority.
- Crossing points aligned with Arcade entrances are to be raised at-grade with footpaths to prioritise pedestrian cross flows at these points.
- The Warners Bay Town Centre Area Plan designates building setbacks from property boundaries to ensure adequate continuous widths of footpath are available and to allow for a private-landscaping/green space interface to the Lane.



**1 Postman's Lane - Typical Plan**  
Scale: 1:300



**2 Postman's Lane - Typical Section**  
Scale: 1:150

## MATERIALS AND FURNITURE SELECTIONS

### FURNITURE

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to *Warners Bay Streetscape Technical Guidelines* for specification guidance on streetscape elements and pavements.

### Bench Seats

**Locations:** At key locations and building entries identified by LMCC at detailed design stage.

### Bike Rail

**Locations:** At key locations along cycle routes, building entries and near outdoor dining zones. Cluster in groups of two or three.

### Waste receptacles- recycling and rubbish bins

**Locations:** At bus stops, taxi stands, along key pedestrian routes and at activity and gathering nodes.

## PAVEMENT

### Main Body Paving

Natural grey concrete, 50-50 mix of black and white aggregate. Exposed aggregate finish.

### Banding

Nom. 200 x 100 x 80mm pavers in a dark grey-Charcoal colour. Lay with long edge perpendicular to line of building frontages.

### Tactile Paving

Integrated TGS1 paving units. Set-out installation to AS1428.4.

### Kerb ramps

Concrete, coloured to provide luminance contrast with the main paving colour in accordance with the *Warners Bay Streetscape Technical Guidelines*.

## Key





## DESCRIPTION

• Applicable to Lymington Way, providing rear through access between John and Charles St. Also provides access to existing loading docks, and potential provision of bus terminus facility.

## OBJECTIVES

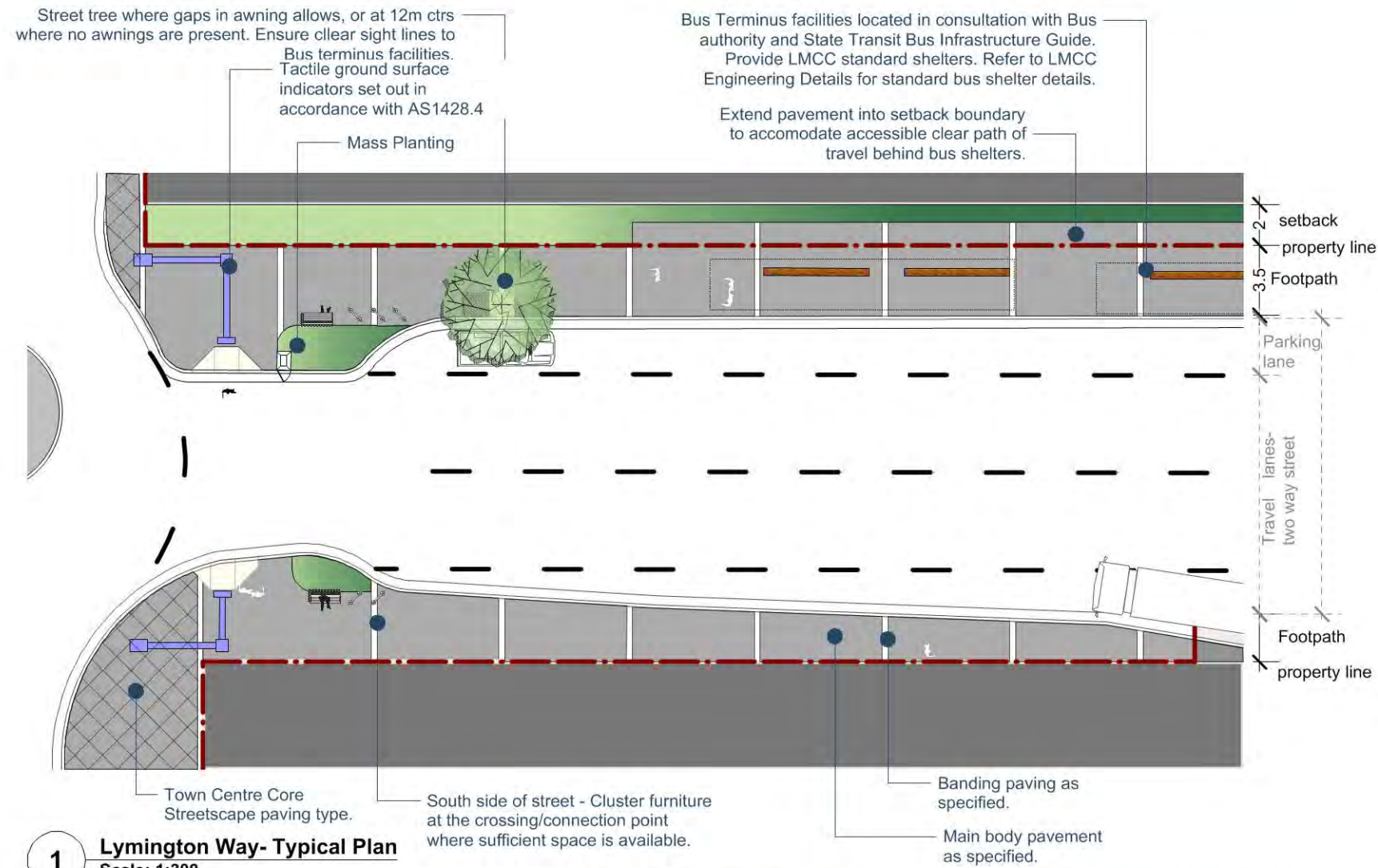
• To provide a safe and accessible connection between John and Charles St.

## EXISTING CONSTRAINTS

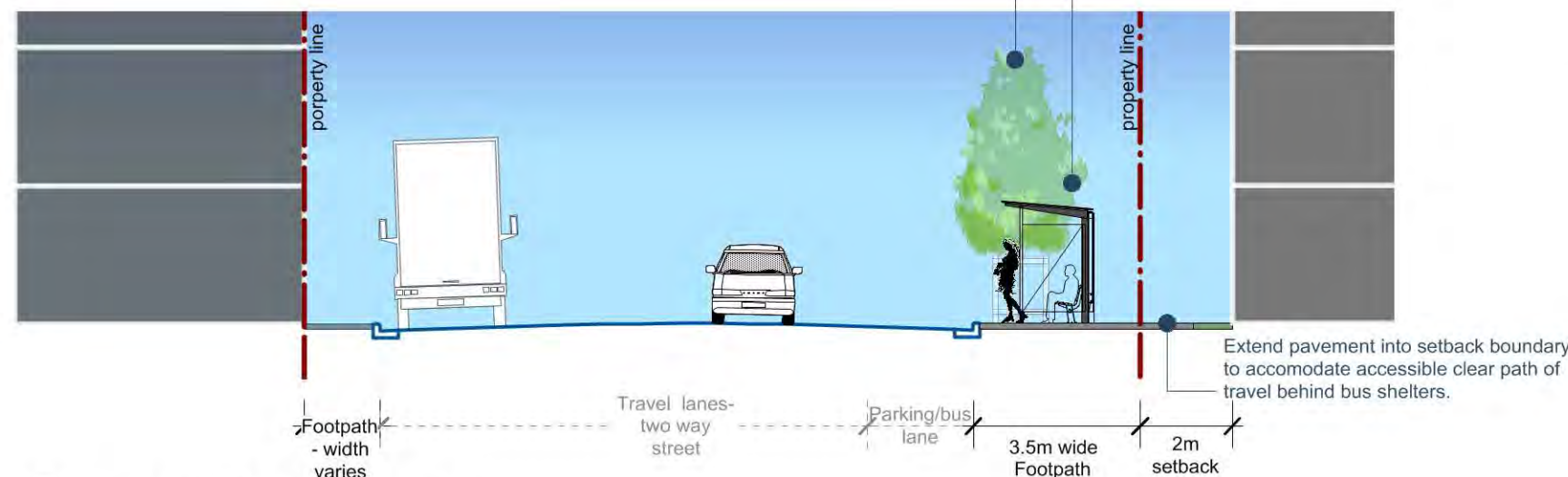
• Currently a one way street, with loading dock ingress/egress.  
• Lymington Way currently reads as a private loading access for the existing development.

## DESIGN RESPONSE

• Provide a two-way street with on street parallel parking in accordance with the Warners Bay Town Centre Area Plan and Framework documents.  
• Accomodate existing loading facilities.  
• Provide a safe and streamlined paved footpath for pedestrian access to parking spaces on the northern side of the street.  
• Provide a safe and streamlined paved footpath for pedestrian through-access to Charles St.



**1 Lymington Way- Typical Plan**  
Scale: 1:300



**2 Lymington Way - Typical Section**  
Scale: 1:150



## MATERIALS AND FURNITURE SELECTIONS

### FURNITURE

The following provide general guidelines for the location of furniture elements. Specific locations for furniture items are to be determined by LMCC at detailed design stage. Additional furniture items may be required for some developments. Refer to Warners Bay Streetscape Technical Guidelines for specification guidance on streetscape elements and pavements.

### Bench Seats

**Locations:** At key locations identified by LMCC at detailed design stage.

### Bike Rail

**Locations:** At key locations along cycle routes, building entries and near outdoor dining zones. Cluster in groups of two or three.

### Waste receptacles- recycling and rubbish bins

**Locations:** At bus stops, taxi stands, along key pedestrian routes and at activity and gathering nodes.

### STREET TREES

**Species selection:** Refer to the Street Tree Plan on Page 14 of this plan.

**Tree Pit construction:** Refer to LMCC Landscape Standard Drawing LSD-PLA-07 Tree Pit in Pavement.

**Tree Guards:** Warners Bay custom Tree Guard- NO decorative panels.

### MASS PLANTING

**Locations:** Adjacent furniture elements where awnings prevent street tree plantings.

**Species:** Native strap-leaved plants, native grasses or groundcovers. Species selection to be determined at detailed design stage. Refer to Warners Bay Streetscape Technical Guidelines for planting, mulching and maintenance requirements.

### PAVEMENT

#### Main Body Paving

Natural grey concrete- broom finish to full width of the footway.

#### Banding

A single row of unit pavers. Paver type as specified in the Warners Bay Streetscape Technical Guidelines. Banding at nominal 6m ctrs but may be varied to align with vertical building elements, building entries, street furniture elements while maintaining a continuous rhythm of banding along the street.

#### Tactile Paving

Integrated TGS1 paving units. Set-out installation to AS1428.4.

#### Kerb ramps

Concrete, coloured to provide luminance contrast with the main paving colour in accordance with the Warners Bay Streetscape Technical Guidelines.



### STREET TREE MASTER PLAN

#### DESCRIPTION

Street trees provide many benefits in an urban environment. They provide shade, reducing heat on the street and surrounding buildings. They provide habitat for native fauna. They change throughout the year with flowers, fruit and leaves, creating visual interest. They create a pleasant environment in which to spend time.

Typical spacings as well as proposed locations for trees within the road reserve have been detailed within the streetscape types, sheets 4 - 10. The locations shown in the typical plans are guidelines only that can be used for planning a development. Actual tree locations will be determined in consultation with LMCC at the time a resident, property owner or developer proposes to install street trees. Allow for at least one tree per lot.

#### OBJECTIVES

- To contribute to a sense of place.
- To improve street microclimates.
- To complement the natural and scenic values of the Town Centre.
- To minimise long term maintenance.

#### EXISTING CONSTRAINTS

- Overhead power lines throughout the town centre
- Undeveloped lots and lack of guttering and footpaths.

#### CRITERIA

- Criteria considered in selecting trees for this Masterplan include:
- Species proven in urban and suburban street environments.
  - Native species.
  - Mature size, height and width to suit anticipated scale of development, awnings and street activity.



Foreshore tree planting is subject to a separate master plan- see Warners Bay Foreshore Master Plan 2012 for details.

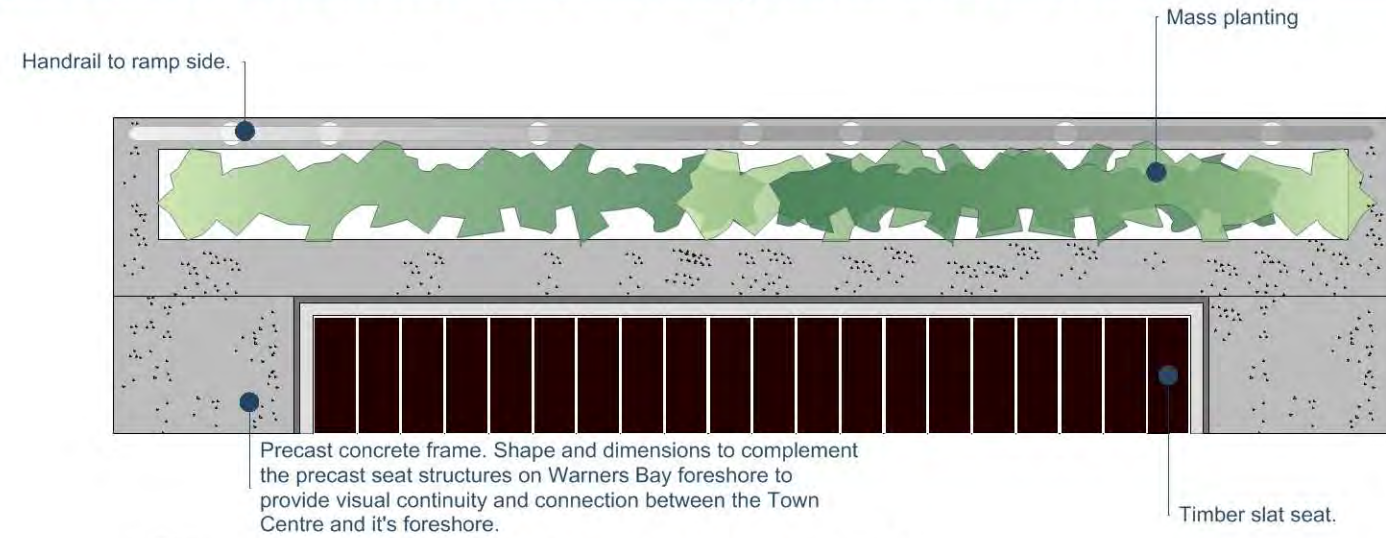
Street trees to the projected kerb pedestrian refuge as part of the treatment proposed for the pedestrian crossing that links the Town Centre to the Foreshore Reserve. Refer to the Dress Circle Streetscape on page 4 of this plan for details of the proposed crossing treatment.

#### Legend

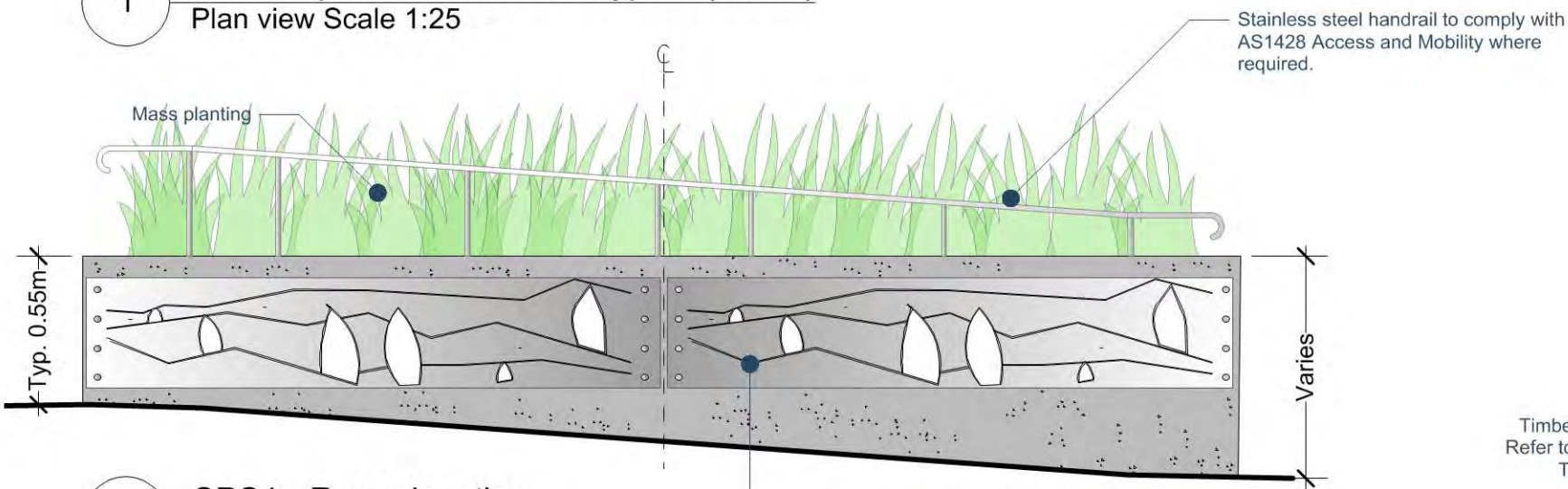
- No Street trees
- Pyrus calleryana* 'Chanticleer' - Callery Pear
- Lagerstroemia indica* 'Sioux' - Pink Crepe Myrtle
- Elaeocarpus reticulatus* - Blueberry Ash
- Callistemon 'Harkness'* - Bottlebrush
- Small tree mix, suitable species -
  - \* *Cupaniopsis anacardioides* - Tuckeroo
  - \* *Tristanopsis laurina* - Water Gum
  - \* *Corymbia ficifolia* 'Wildfire' - Flowering gum



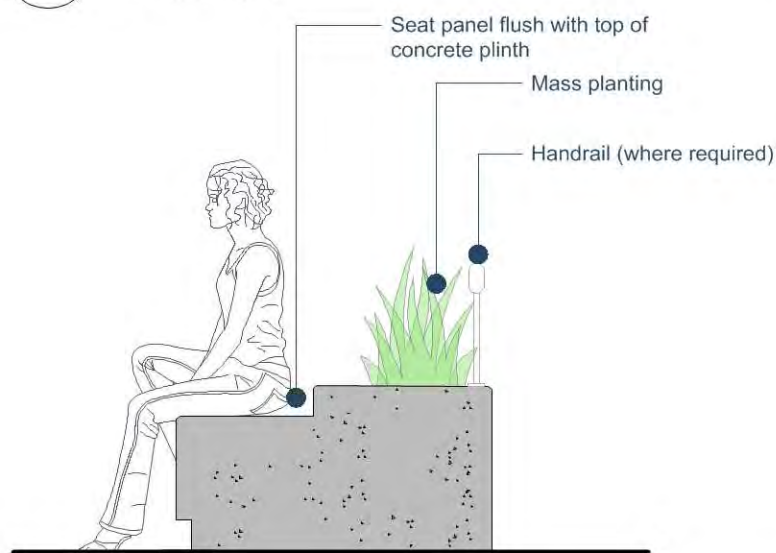




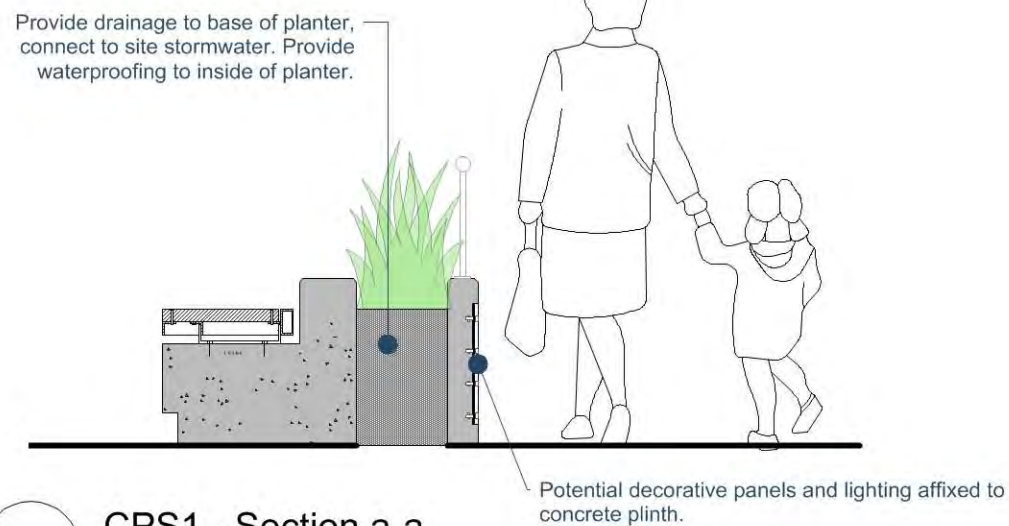
1 Custom planter and seat - Type 1 (CPS1)  
Plan view Scale 1:25



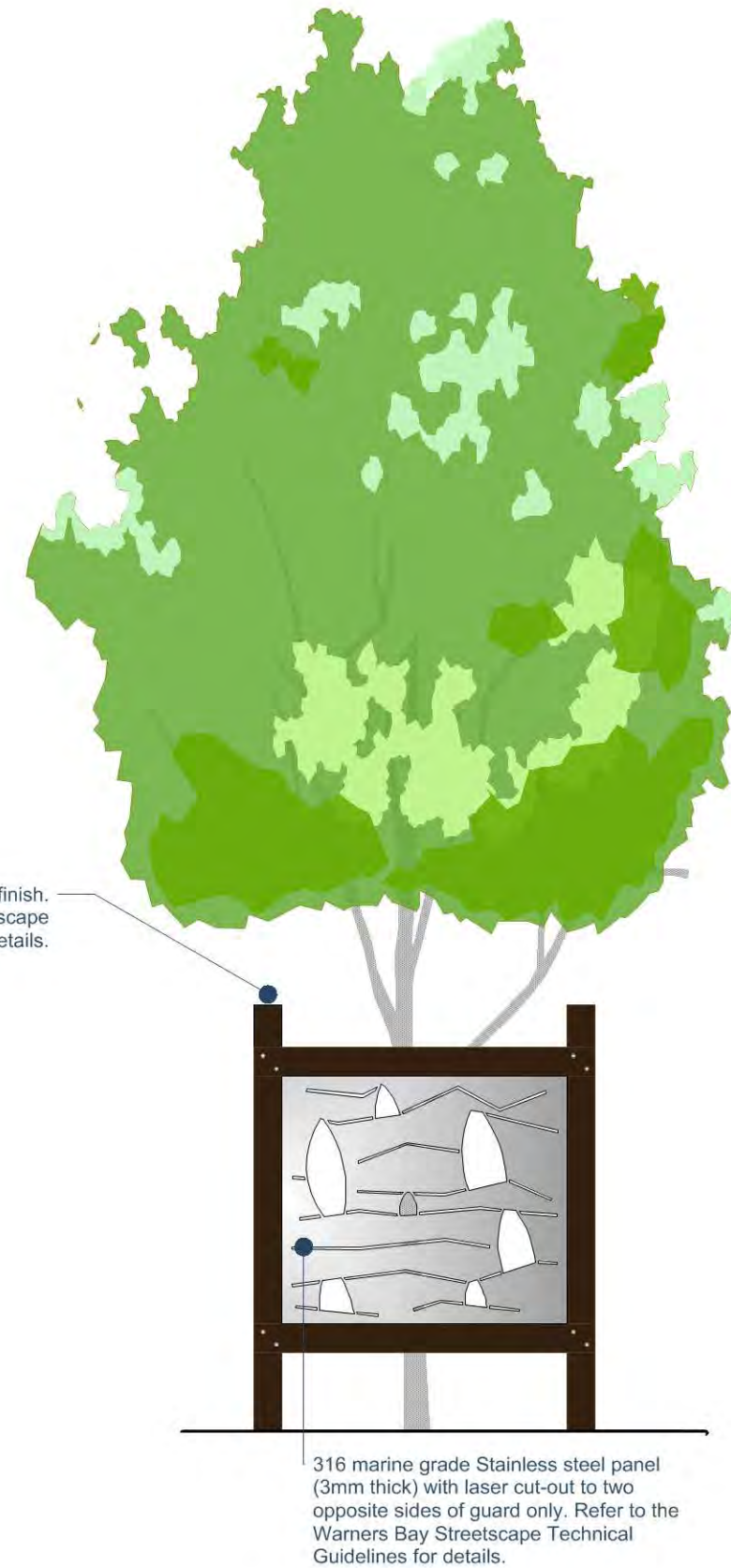
2 CPS1 - Rear elevation  
Scale: 1:25



3 CPS1 - Side elevation  
Scale: 1:25

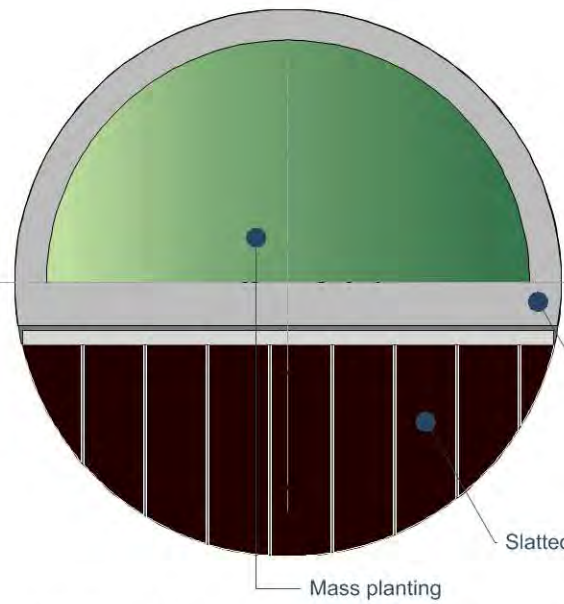


4 CPS1 - Section a-a  
Scale: 1:25



5 Tree guard - elevation  
Scale: 1:25





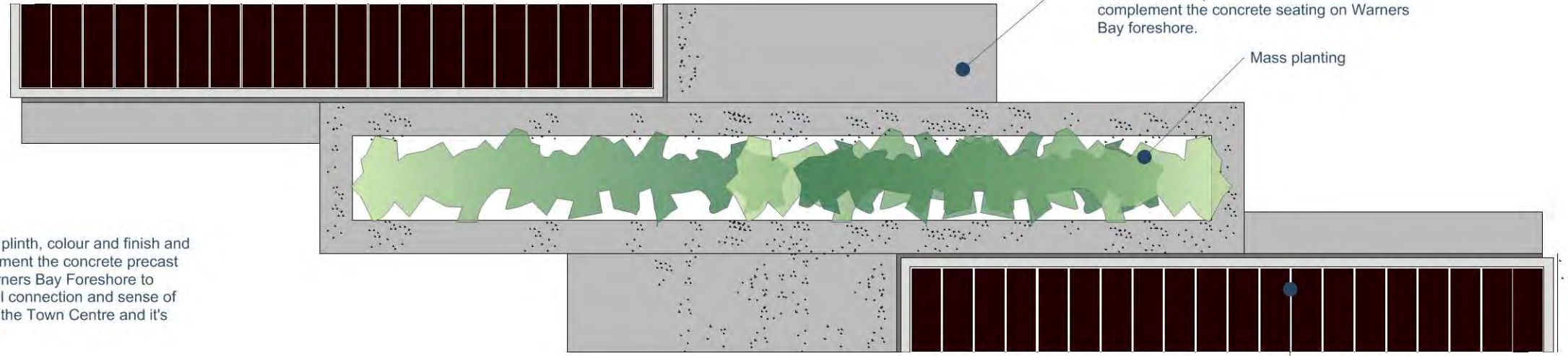
Insitu concrete plinth, colour and finish and form to complement the concrete precast seating on Warners Bay Foreshore to provide a visual connection and sense of place between the Town Centre and it's Foreshore.

Slatted seating area.

Mass planting

**1 Custom planter and seat - Type 2 (CPS2)**

Scale: 1:25



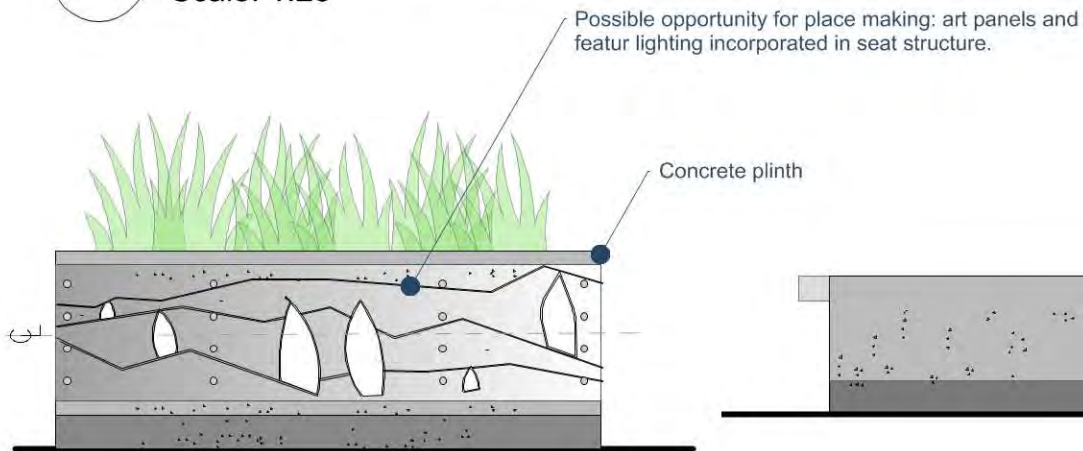
Insitu concrete plinth, colour and finish to complement the concrete seating on Warners Bay foreshore.

Mass planting

Slatted seat area.

**5 Custom planter box and seat - Type 3 (CPS3)**

Scale: 1:25



Possible opportunity for place making: art panels and featur lighting incorporated in seat structure.

Concrete plinth

**2 CPS2 - Rear elevation**

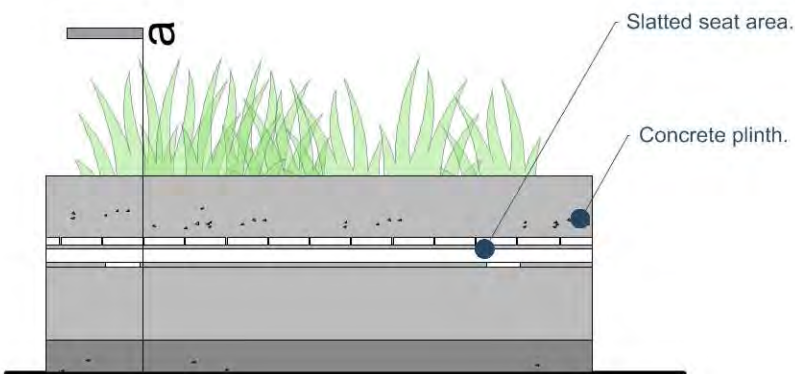
Scale: 1:25



Possible opportunity for place making: art panels and featur lighting incorporated in seat structure.

**6 CPS3 - Elevation**

Scale: 1:25

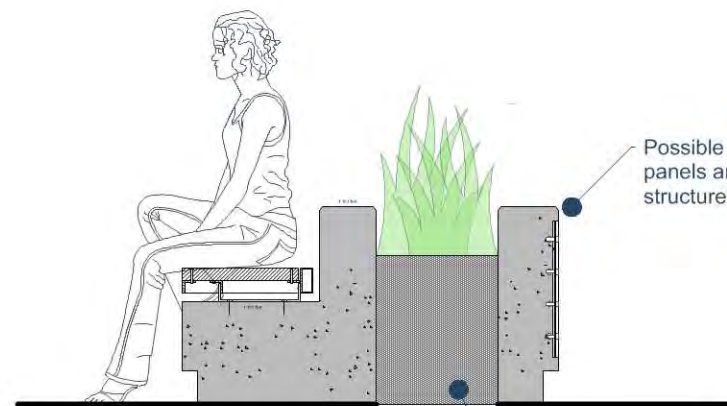


Slatted seat area.

Concrete plinth.

**3 CPS2 - Front elevation**

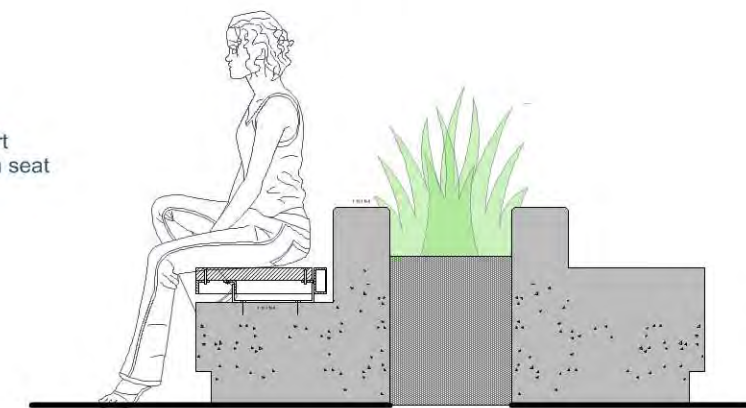
Scale: 1:25



Possible opportunity for place making: art panels and featur lighting incorporated in seat structure.

**4 CPS2 - Section a-a**

Scale: 1:25



Provide drainage to base of planter, connect to site stormwater. Provide waterproofing to inside of planter. Refer to Warners Bay Streetscape Technical Guidelines for details of planter box and soil specification.

**7 CPS3 - Section a-a**

Scale: 1:25



